## PARKING STUDY

For

## City of Miami Beach

Design-Build Services for West Avenue Improvements
Phase II North of 14 St. and Phase II South of 14 St.
Project Nos. M-16002.001 and M-16002.002

#### **Note by Stephan Ginez:**

### **Loss Parking space North Project:**

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Total loss: 144

## PREPARED BY:



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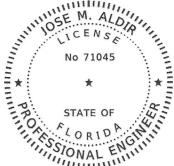
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CWI PROJECT NO.: 17102.00 & 17105.00

## **MAY 2022**

This item has been digitally signed and sealed by Jose M. Aldir, P.E. on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies



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**Appendix 1 – Parking Spreadsheets** 

Appendix 2 – Parking Restriction Standards

#### 1. Introduction

West Avenue Phase II improvements is a design build project that will raise the elevation of West Ave, Bay Road, and all the adjacent roads within the West Avenue Neighborhood, including from 8 Street to Lincoln Road and from west of Alton Road to Biscayne Bay. The purpose of this project is to reduce the risk within the neighborhood. The project will include total reconstruction of the roadways, underground utility infrastructure improvements, a new drainage system, and a new street lighting system.

## 2. Purpose of the Study

Due to the reconstructed roadways, the signing and pavement markings will be replaced according to the approved roadway typical sections and to meet current City of Miami Beach, MUTCD, and County standards. The new roadway typical sections and pavement markings will affect the number of parking spaces within the affected areas. A parking study was requested by the City to compare the quantity of existing parking against the quantity of parking that will be provided by the reconstruction project. The City stipulated that the study should include the actual linear feet of existing parking as well as a count of the actual number of parked vehicles within the study area. The study should also include reasons for lost parking and possible mitigation strategies that could be used to regain some of the lost parking. The study area limits are listed below:

Street Name	From	То
West Avenue	6 Street	Lincoln Road
Bay Road	14 Street	Collins Canal
Lincoln Court	Lincoln Road	Collins Canal
Lincoln Road	Biscayne Bay	Alton Road
Lincoln Terrace	Biscayne Bay	Bay Road
16 Street	Biscayne Bay	Alton Road
15 Terrace	Bay Road	Alton Road
15 Street	Bay Road	Alton Road
Flamingo Way	Bay Road	West Avenue
14 Court	West Avenue	Alton Road
14 Terrace	Bay Road	West Avenue
14 Street	Biscayne Bay	Alton Road
13 Street	West Avenue	Alton Road
12 Street	West Avenue	Alton Road
11 Street	West Avenue	Alton Road
10 Street	West Avenue	Alton Road
9 Street	West Avenue	Alton Road
8 Street	West Avenue	Alton Road

## 3. Executive Summary

West Avenue Phase II improvements is a design build project that will raise the elevation of West Ave, Bay Road, and all the adjacent roads within the West Avenue Neighborhood between Biscayne Bay and Alton Road. Raising the elevation of the roadways and sidewalks is necessary to mitigate roadway deterioration and flooding caused by king tides and sea level rise. The higher roadway and sidewalk elevations will ensure that the roadways and sidewalks stay dry and walkable.

The roadway reconstruction concepts in this project were developed in close coordination with the residents of the area, including the NAVNA and WAVNA Neighborhood Associations. The reconstruction of the roadways provided an opportunity to improve the walkability and ambiance of the neighborhood by incorporating features such as bike lanes, traffic calming, tree canopy, decorative pavers, decorative lighting, and other features.

The North project study area currently has approximately 6661 LF of parking, 1428 LF of which violates current parking restrictions. The proposed current design provides approximately 4319 LF of parking. The approximate loss of parking between the existing (meeting current parking restrictions) and proposed is 6661 LF – 1428 LF -4319 LF = 914 LF. Of the 914 LF of lost parking, 474 LF is due to the addition of bike lanes.

The South project study area currently has approximately 3075 LF of parking, 735 LF of which violates current parking restrictions. The proposed current design provides approximately 1142 LF of parking. The approximate loss of parking between the existing (meeting current parking restrictions) and proposed is 3075 LF – 735 LF – 1142 LF = 1198 LF. Of the 1198 LF of lost parking, 1011 LF is due to the addition of bike lanes.

A comparison of lost parking spaces is shown below converting the lineal feet of parking shown above into 22-ft long parking spaces.

Description	West Ave North	West Ave South
Existing LF	6661 LF	3075 LF
LF in Violation of standards	1428 LF	735 LF
Existing if 22 Ft Spaces	302 spaces	139 spaces
22 Ft Spaces in Violation	64 spaces	33 spaces
Existing "Legal" spaces	238 "Legal" spaces	106 "Legal" spaces
Proposed with 22 Ft spaces	196 spaces	51 spaces
Total Lost Parking in prop design	Total 42 spaces lost	Total 55 spaces lost
Lost Parking due to Bike Lane	21 spaces	45 spaces
Lost Parking not related to Bike Lane	21 spaces	10 spaces
Percent of Parking Lost	17.6%	51.8%

Proposed Parking loss is due to the Bike Lanes, additional landscaping, fire hydrants, and other design restrictions.

Mitigation strategies that can be implemented to recapture some of the lost parking include modifications of the proposed landscape, relocation of fire hydrants, and modifications of the proposed bike lanes, however further studies would be required to confirm if they are feasible and to determine how much parking can be recaptured. This study noted several CitiBike Stations that will be removed. The proposed design does not include any CitiBike Stations.

## 4. Data Acquisition Methodology

CWI assigned the data collection to a two-person team. The team performed a daytime field visit to measure the linear feet of existing public parking along all of the listed roads. The daytime field visit took place on Tuesday April 12, 2022 between the hours of 7:00 AM and 5:00 PM. A measuring wheel was used to measure the parking areas and the measurements were noted on 11" x 17" plans. The actual vehicles legally parked within the study limits were counted on Thursday April 14, 2022 between the hours of 8:00 PM and 11:00 PM. The results were noted on 11" x 17" plans.

Also noted were areas where individual parking spaces were delineated, special vehicle parking areas, i.e., motorcycle parking, areas identified as loading zones, special utilization areas, and others.

The field data was transferred to spreadsheets (one for the North project and one for the South project) which list all of the roadway segments within the study area. The proposed linear feet of parking and number of proposed marked parking spaces according to the latest signing and pavement marking plans on file were added to the spreadsheets. These spreadsheets are included in Appendix 1.

Information taken from the parking data was used to summarize the results of the parking study, along with reasons for parking losses and possible mitigation strategies that may be used to recapture parking. The study results are listed in the following section and are summarized street by street and segment by segment. Please refer to the Data Spreadsheets in Appendix 1 for more detailed information when needed.

## 5. Study Results

This section contains a street by street, segment by segment summary of the net change in parking, reasons for the change, and mitigation that may allow the recapture of some parking. The North Project is listed first, followed by the South Project.

## **NORTH PROJECT**

## 14th Street between Biscayne Bay and Bay Road

There is a net loss of approximately 74 LF of existing parking. A review of the existing parking identified approximately 18 LF of parking that should not exist due to violations of clear sight restrictions at intersections. Removal of the landscape areas would provide space for additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
74	2	0	0

LOSS :

## 14th Street between Bay Road and West Avenue

The West Avenue Improvements project does not include this segment. Therefore, no changes to parking are proposed. A review of the existing parking identified approximately **5 LF** of parking that should not exist due to the driveway safe sight triangle requirement.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
288	10	N/A	N/A

LOSS;

## 14th Street between West Avenue and Alton Road

The West Avenue Improvements project does not include this segment. Therefore, no changes to parking are proposed. A review of the existing parking identified approximately **40 LF** of parking that should not exist due to sight restrictions at intersections (30 LF) and driveway safe sight triangle restrictions (10 LF).

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
216 (Parallel) 158 (Angle)	19	N/A	N/A

LOSS:

## 14th Terrace between Bay Road and West Avenue

There is no existing or proposed parking along this segment.

#### 14th Court between West Avenue and Alton Road

There is a net loss of approximately 92 LF of existing parking. A review of the existing parking identified **60 LF** of parking that should not exist due to sight restrictions at intersections (55 LF) and driveway safe sight triangle restrictions (5 LF). The proposed parking is maximized and there are no mitigation strategies identified to increase parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
444	18	352	16
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## Flamingo Way between Bay Road and West Avenue

There is a net loss of 37 LF of existing parking. A review of the existing parking identified 25 LF of parking that should not exist due to sight restrictions at intersections. Removal of the landscape areas would provide space for additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
37	1	0	0

## 15th Street between Bay Road and West Avenue

There was a net loss of approximately 47 LF of existing parking. A review of the existing parking identified 43 LF of parking that should not exist due to sight restrictions at intersections (13 LF) and driveway safe sight triangle restrictions (30 LF). The proposed parking is maximized and there are no mitigation strategies identified to increase parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
269	14	222	10

## 15th Street between West Avenue and Alton Road

There was a net loss of approximately 98 LF of existing parking. A review of the existing parking identified 92 LF of parking that should not exist due to sight restrictions at intersections (70 LF) and driveway safe sight triangle restrictions (22 LF). The proposed parking is maximized and there are no mitigation strategies identified to increase parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
380 (Parallel) 10 (Perpendicular)	13 Cars & 6 Motorcycles	292	13

## 15th Terrace between Bay Road and West Avenue

There was a net loss of approximately 138 LF of existing parking. A review of the existing parking identified 131 LF of parking that should not exist due to sight restrictions at intersections (35 LF) and driveway safe sight triangle restrictions (96 LF). Removal of the landscape areas would provide space for additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
182	9	44	2
			LOSS

#### 15th Terrace between West Avenue and Alton Road

There was a net loss of approximately 103 LF of existing parking. A review of the existing parking identified 90 LF of parking that should not exist due to sight restrictions at intersections (54 LF) and driveway safe sight triangle restrictions (36 LF). The proposed parking is maximized and there are no mitigation strategies identified to increase parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
402 (parallel) 9 (perpendicular)	20 Cars & 4 Motorcycles	308	13

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## 16th Street between Biscayne Bay and Bay Road

The parking along this segment is perpendicular, private parking that is not being modified by this West Avenue Improvements project. Therefore, no changes to parking are proposed.

Existing Number of Perpendicular Parking Spaces

## 16th Street between Bay Road and West Avenue

LOSS: 0

There was a net gain of approximately 52 LF of parking. The area currently occupied by Citi Bike Station #156 is being repurposed as proposed parking. A review of the existing parking identified 114 LF of parking that should not exist due to sight restrictions at intersections (35 LF) and driveway safe sight triangle restrictions (79 LF). There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
282	15 Cars & 1 Motorcycle	230	10

Loss: 5

#### 16th Street between West Avenue and Alton Road

There was a net loss of approximately 90 LF of existing parking. A review of the existing parking identified 60 LF of parking that should not exist due to sight restrictions at intersections (35 LF) and driveway safe sight triangle restrictions (25 LF). There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
334	14	244	11

Loss: 3

## Lincoln Terrace between Biscayne Bay and Bay Road

There was a net loss of approximately 25 LF of existing parking. A review of the existing parking identified 50 LF of parking that should not exist due to sight restrictions at intersections. There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
463	19	438	N/A

## Lincoln Road between Biscayne Bay and Lincoln Court

There was a net loss of approximately 86 LF of existing parking. A review of the existing parking identified 49 LF of parking that should not exist due to sight restrictions at intersections (39 LF) and driveway safe sight triangle restrictions (10 LF). There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
131 (Perpendicular)	14	21 (Parallel) 24 (Angle)	3
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## Lincoln Road between Lincoln Court and Bay Road

There was a net loss of approximately 38 LF of existing parking. A review of the existing parking identified **93 LF** of parking that should not exist due to sight restrictions at intersections (75 LF) and driveway safe sight triangle restrictions (18 LF). There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
44 (Parallel) 97 (Perpendicular)	9	21 (Parallel) 82 (Angle)	8

LOSS:

## Lincoln Road between Bay Road and West Avenue

There was a net loss of approximately 200 LF of existing parking. A review of the existing parking identified **75** LF of parking that should not exist due to sight restrictions at intersections (55 LF) and driveway safe sight triangle restrictions (20 LF). There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
235 (Parallel) 236 (Angle)	22	271 (Angle)	23

GAIN: +1

#### Lincoln Road between West Avenue and Alton Road

There was a net loss of approximately 82 LF of existing parking. A review of the existing parking identified **73** LF of parking that should not exist due to sight restrictions at intersections (45 LF) and driveway safe sight triangle restrictions (28 LF). There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
151 (Parallel) 202 (Angle)	14 Cars & 3 Motorcycles	84 (Parallel) 187 (Angle)	19

GaIN: +5

## Lincoln Court between Lincoln Road and Collins Canal

There is no existing or proposed parking within this segment.

## Bay Road between 14th Street and 14th Terrace

There was no net gain or loss of parking within this segment.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces		
154	5	154	7		0
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## Bay Road between 14th Terrace and Flamingo Way

There was a net loss of approximately 48 LF of existing parking. A review of the existing parking identified 15 LF of parking that should not exist due to sight restrictions at intersections. There are existing above ground utility features (vaults, wells) that occupy areas that could potentially be used for parking. There are no mitigation strategies identified that can be used to recapture parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
178	8	130	6

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## Bay Road between Flamingo Way and 15th Street

There was a net gain of approximately 38 LF of parking. The area currently occupied by Citi Bike Station #154 is being repurposed as proposed parking. A review of the existing parking did not identify parking in violation of current standards. There may be opportunities to remove landscape areas to accommodate additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
185	10	223	10

LOSS 10

## Bay Road between 15th Street and 15th Terrace

There was no change in LF of parking. A review of the existing parking identified 36 LF of parking that should not exist due to sight restrictions at intersections. There may be opportunities to remove landscape areas to accommodate additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
219	10 Cars & 5 Motorcycles	219	10

Loss:

## Bay Road between 15th Terrace and 16th Street

There was a net loss of approximately 69 LF of parking. A review of the existing parking identified 27 LF of parking that should not exist due to sight restrictions at intersections. There are no mitigation strategies identified that can be implemented to add parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
175	8 Cars & 1 Motorcycle	106	5

LOSS 3

Bay Road between 16th Street and Lincoln Terrace

There was a net loss of approximately 84 LF of parking. A review of the existing parking identified 26 LF of parking that should not exist due to sight restrictions at intersections (7 LF) and driveway safe sight triangle restrictions (19 LF). There are no mitigation strategies identified that can be implemented to add parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
106	5	22	1
100			LOSS

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## Bay Road between Lincoln Terrace and Lincoln Road

There was a net loss of approximately 102 LF of parking. A review of the existing parking identified 97 LF of parking that should not exist due to sight restrictions at intersections (42 LF) and driveway safe sight triangle restrictions (55 LF). There are no mitigation strategies identified that can be implemented to add parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
218	17	116	5

## Bay Road between Lincoln Road and Collins Canal

There was a net loss of approximately 293 LF of parking. A review of the existing parking identified 190 LF of parking that should not exist due to sight restrictions at intersections (32 LF) and driveway safe sight triangle restrictions (187 LF). There are no mitigation strategies identified that can be implemented to add parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
437	20 Cars & 6 Motorcycles	144	7

LOSS: 13

## West Avenue between 14th Street and 14th Terrace

There was a net loss of approximately 104 LF of parking. A review of the existing parking identified 54 LF of parking that should not exist due to sight restrictions at intersections (25 LF) and driveway safe sight triangle restrictions (29 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
192	7 Cars & 3 Motorcycles	88	4

## West Avenue between 14th Terrace and Flamingo Way

There was a net loss of approximately 63 LF of parking. A review of the existing parking identified 10 LF of parking that should not exist due to driveway safe sight triangle restrictions. There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
63	3	0	0

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## West Avenue between Flamingo Way and 15th Street

There was a net loss of approximately 167 LF of parking. A review of the existing parking identified **45** LF of parking that should not exist due to sight restrictions at intersections (38 LF) and driveway safe sight triangle restrictions (7 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
314	15	147	7

LOSS:

## West Avenue between 15th Street and 15th Terrace

There was a net loss of approximately 192 LF of parking. A review of the existing parking identified **35** LF of parking that should not exist due to sight restrictions at intersections (25 LF) and driveway safe sight triangle restrictions (10 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
320	15 Cars & 3 Motorcycles	128	6

280\_

## West Avenue between 15th Terrace and 16th Street

There was a net loss of approximately 96 LF of parking. A review of the existing parking identified 10 LF of parking that should not exist due to driveway safe sight triangle restrictions. current standards (list the standard). There are no mitigation strategies that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
118	5	22	1

LOSS

## West Avenue between 16th Street and Lincoln Road

There is no existing or proposed parking along this segment.

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## **SOUTH PROJECT**

### 8th Street between West Avenue and Alton Road

There is a net loss of approximately 158 LF of existing parking. A review of the existing parking identified 107 LF of parking that should not exist due to sight restrictions at intersections (82 LF) and driveway safe sight triangle restrictions (25 LF). There are no mitigation strategies identified that can be implemented to add parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
436	21	278	12

## 9th Street between West Avenue and Alton Road

There is a net loss of approximately 88 LF of existing parking. A review of the existing parking identified **45** LF of parking that should not exist due to sight restrictions at intersections (35 LF) and driveway safe sight triangle restrictions (10 LF). The only mitigation identified for this segment is the relocation of the fire hydrant to an area within a parking restriction zone. Further analysis is required to determine if relocation of the fire hydrant is feasible.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
288 (Parallel) 20 (Perpendicular)	11 Cars & 1 Motorcycle	220	9

## 10th Street between Biscayne Bay and West Avenue

There is a net loss of approximately 119 LF of existing parking. A review of the existing parking identified **30** LF of parking that should not exist due to sight restrictions at intersections. Removal of the landscape areas would provide space for additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
119 (Perpendicular)	13	0	0

## 10th Street between West Avenue and Alton Road

There is no proposed work within this segment. The existing parking was documented as part of the data collection.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces N/A
61 (Parallel)	8 Cars & 18 Motorcycles	N/A	
28 (Perpendicular) 107 (Angle)			

### 11th Street between West Avenue and Alton Road

There is a net loss of approximately 88 LF of existing parking. A review of the existing parking identified **75** LF of parking that should not exist due to sight restrictions at intersections (60 LF) and driveway safe sight triangle restrictions (15 LF). Removal of the landscape areas would provide space for additional parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
259 (Parallel) 7 (Perpendicular)	5	178	7 .

#### 12th Street between West Avenue and Alton Road

There is a net loss of approximately 153 LF of existing parking. The area currently occupied by Citi Bike Station #143 is being repurposed as proposed landscaping and parking. A review of the existing parking identified 150 LF of parking that should not exist since it blocks perpendicular parking in private property. Approximately **45 LF** of existing parking should not exist due to sight restrictions at intersections (35 LF) and driveway safe sight triangle restrictions (10 LF).

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
394	15	241	10

## 13th Street between West Avenue and Alton Road

There is a net loss of approximately 112 LF of existing parking. A review of the existing parking identified **78** LF of parking that should not exist due to sight restrictions at intersections (43 LF) and driveway safe sight triangle restrictions (35 LF). The proposed parking is maximized and there are no mitigation strategies identified to increase parking.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
250 (Parallel) 87 (Angle)	18	177 (Parallel) 48 (Angle)	12

### West Avenue between 6th Street and 8th Street

This segment is not included in the West Avenue Improvements project scope. The existing parking was documented to comply with the City's request.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
261 (Parallel) 25 (Angle)	12 Cars & 1 Motorcycle	N/A	N/A

## West Avenue between 8th Street and 9th Street

There is a net loss of approximately 144 LF of existing parking. A review of the existing parking identified 15 LF of parking that should not exist due to sight restrictions at intersections (5 LF) and driveway safe sight triangle restrictions (10 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
144	6 Cars & 11 Motorcycles	0	0

## West Avenue between 9th Street and 10th Street

There is a net loss of approximately 332 LF of existing parking. A review of the existing parking identified **37** LF of parking that should not exist due to sight restrictions at intersections. There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
310 (Parallel) 22 (Perpendicular)		0	0
310 (Farallel) 22 (1 elperiolodial)	14 Outs a 11 Motorby orde		

## West Avenue between 10th Street and 11th Street

There is a net loss of approximately 190 LF of existing parking. A review of the existing parking identified **20** LF of parking that should not exist due to sight restrictions at intersections (15 LF) and driveway safe sight triangle restrictions (5 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

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Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
190	7 Cars & 16 Motorcycles	0	0
190	1 Galo a 10 Motorejero		

## West Avenue between 11th Street and 12th Street

There is a net loss of approximately 209 LF of existing parking. A review of the existing parking identified **35** LF of parking that should not exist due to sight restrictions at intersections (20 LF) and fire hydrant parking restrictions (15 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
209	4 Cars & 18 Motorcycles	0	0

## West Avenue between 12th Street and 13th Street

There is a net loss of approximately 132 LF of existing parking. A review of the existing parking identified **38** LF of parking that should not exist due to sight restrictions at intersections (13 LF) and driveway safe sight triangle restrictions (25 LF). There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
132	6 Cars & 12 Motorcycles	0	0

## West Avenue between 13th Street and 14th Street

There is a net loss of approximately 208 LF of existing parking. A review of the existing parking identified **60 LF** of parking that should not exist due to driveway safe sight triangle restrictions. There are no mitigation strategies identified that can be implemented to add parking based on the required bike facilities.

Existing Parking LF	Actual Parked Vehicles	Proposed Parking LF	Proposed Parking Spaces
208	7	0	0

## 6. Summary

West Avenue Phase II improvements is a design build project that will raise the elevation of West Ave, Bay Road, and all the adjacent roads within the West Avenue Neighborhood between Biscayne Bay and Alton Road. Raising the elevation of the roadways and sidewalks is necessary to mitigate roadway deterioration and flooding caused by king tides and sea level rise. The higher roadway and sidewalk elevations will ensure that the roadways and sidewalks stay dry and walkable.

The roadway reconstruction concepts in this project were developed in close coordination with the residents of the area, including the West Avenue Neighborhood Association. The reconstruction of the roadways provided an opportunity to improve the walkability and ambiance of the neighborhood by incorporating features such as bike lanes, traffic calming, tree canopy, decorative pavers, decorative lighting, and other features.

The North project study area currently has approximately 6661 LF of parking, 1428 LF of which violates current parking restrictions. The proposed current design provides approximately 4319 LF of parking. The approximate loss of parking between the existing (meeting current parking restrictions) and proposed is 6661 LF - 1428 LF -4319 LF = 914 LF. Of the 914 LF of lost parking, 474 LF is due to the addition of bike lanes.

The South project study area currently has approximately 3075 LF of parking, 735 LF of which violates current parking restrictions. The proposed current design provides approximately 1142 LF of parking. The approximate loss of parking between the existing (meeting current parking restrictions) and proposed is 3075 LF - 735 LF - 1142 LF = 1198 LF. Of the 1198 LF of lost parking, 1011 LF is due to the addition of bike lanes.

Mitigation strategies that can be implemented to recapture some of the lost parking include modifications of the proposed landscape, relocation of fire hydrants, and modifications of the proposed bike lanes, however further studies would be required to confirm if they are feasible and to determine how much parking can be recaptured. This study noted several CitiBike Stations that will be removed. The proposed design does not include any CitiBike Stations.

# APPENDIX 1 Parking Spreadsheets

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Civil Works Inc	rike Inc				neophase continues (neophysical)		WES	WEST AV	ENUE	IMPE	VENUE IMPROVEMENTS	ENTS	S - 8	- NORTH PROJECT	PROJ	ECT								May 10, 2020
				-			EXIS	EXISTING				-	-	PROPOSED	SED									
						Type o	Type of Parking							Type of Parking	arking				Net G	ain/Loss or	Net Gain/Loss of Parking [LF]			
Location	Street		;	Par	Parallel	Perpe	Perpendicular	Angle	şle	Spaces	Spaces Occupied	Parallel	lel	Perpendicular	icular	Angle	le e	Parallel	el	Perpendicular	icular	Angle	e	
Number	Name	Limits	Side	5	# of Marked Spaces	5	# of Marked Spaces	1	# of Marked Spaces	Cars	Motorcycle	5	# of Marked Spaces	<u>-</u>	# of Marked Spaces	J.	# of Marked Spaces	5	# of Marked Spaces	5	# of Marked Spaces	5	# of Marked Spaces	Comments
20	14 Ct	Between West Ave and	South	100	4					25		87	4					-13	0					1 Proposed Disabled Space.
21	14 Ct	Between West Ave and	South			21	2			0				0	0					-21	-5			2 Existing Motorcycle Spaces.
23	14 Ct	Between West Ave and	South	104	4					9		84	4					-20	0					
SOL	BTOTAL SC BETWEEN W	SUBTOTAL SOUTH SIDE OF 14 COURT= BETWEEN WEST AVE AND ALTON ROAD	RT=	204	80	21	2	0	0	11	0	171	œ	0	0	0	0	.33	0	-21	-2	0	0	Proposed Disabled Space.     Existing Motorcycle     Spaces.
22	14 Ct	Between West Ave and	North	100	2					60		94	4					φ	÷					1 Proposed Disabled Space.
24	14 Ct	Between West Ave and	North	59	2					2		87	4					28	2					
75	14 Ct	Alton Koad Between West Ave and	North	09	3					2		0	0					09-	ę.					<ol> <li>Existing Freight Loading Zone.</li> </ol>
SOIL	BTOTAL NO	SUBTOTAL NORTH SIDE OF 14 COURT= BETWEEN WEST AVE AND ALTON ROAD	RT=	219	10	0	0	0	0	7	0	181	80	0	0	0	0	-38	-5	0	0	0	0	proposed Handicap and 3 existing loading zone spaces spaces
1	14 St	Between Biscayne Bay and	South	44	2					1		0	0					-44	-2					
2	14 St	Between Biscayne Bay and Bay Road	North	30	1					1		0	0					-30	Ţ.					
	SUBT	SUBTOTAL 14 STREET= Between Biscayne Bay and Bay Road		74	m	0	0	0	0	7	0	0	0	0	0	0	0	-74	φ	0	0	0	0	
3	14 St	Between Bay Road and	South	88	4					3		N/A	N/A					N/A	N/A					
4	14 St	Between Bay Road and West Ave	South	44	2					1		N/A	N/A					N/A	N/A					
1S*	JBTOTAL S BETWEEN	*SUBTOTAL SOUTH SIDE OF 14 STREET= BETWEEN BAY ROAD AND WEST AVE	ET=	132	9	0	0	0	0	4	0	N/A	N/A	0	0	0	0	N/A	N/A	0	0	0	0	
5	14 St	Between Bay Road and West Ave	North	68	4					т		N/A	N/A					N/A	N/A					
9	14 St	Between Bay Road and West Ave	North	45	2					2		N/A	N/A					N/A	N/A					
7	14 St	Between Bay Road and West Ave	North	22	н					н		N/A	A/N					N/A	N/A					
15*	JBTOTAL P BETWEEN	*SUBTOTAL NORTH SIDE OF 14 STREET=	{EET=	156	7	0	0	0	0	9	0	N/A	N/A	0	0	0	0	N/A	N/A	0	0	0	0	
80	14 St	Between West Ave and Alton Road	South					7	1	0						N/A	N/A					N/A	N/A	Motorcycle
6	14 St	Between West Ave and	South					65	'n	S						N/A	N/A					N/A	A/A	
10	14 St	Between West Ave and Alton Road	South					11	1	1						N/A	N/A					N/A	N/A	
12	14 St	Between West Ave and Alton Road	South	109	5					4		N/A	N/A					N/A	N/A					
	Popor.	Thursday At. 14, 2002 from 8:00 PM to 11:00 PM.	Thursday 04-3	4-2022 fre	M 8:00 PM	to 11:00 P	M.																	1000

Civil Me	Civil Works, Inc.						WEST A		NUE	IMPR	VENUE IMPROVEMENTS	ENTS		- NORTH PROJECT	ROJ	CT								May 10, 2020
	our cours						EXIS							PROPOSED	SED									
						Type of Parking	Parking							Type of Parking	arking				Net G	ain/Loss of	Net Gain/Loss of Parking [LF]	Ы		
Location	Street	4	9	Parallel	lel	Perpendicular	licular	Angle	9	spaces Occupied	nccupied	Parallel	lel	Perpendicular	cular	Angle		Parallel	-	Perpendicular	cular	Angle	le	4
Number	Name		appio	5	# of Marked Spaces	5	# of Marked Spaces	5	# of Marked Spaces	Cars	Motorcycle	7	# of Marked Spaces	F .	# of Marked Spaces	<u> </u>	# of Marked Spaces	5	# of Marked Spaces	<u>u</u>	# of Marked Spaces	5	# of Marked Spaces	Comments
us*	BTOTAL S	*SUBTOTAL SOUTH SIDE OF 14 STREET= BETWEEN WEST AVE AND ALTON ROAD	=13	109	r	0	0	83	7	10	0	N/A	N/A	0	0	N/A	N/A	N/A	N/A	0	0	N/A	N/A	
11	14 St	Between West Ave and Alton Road	North					75	7	ī.						N/A	N/A					N/A	N/A	
13	14 St	Between West Ave and Alton Road	North	107	ī.					4		N/A	N/A					N/A	N/A					
ns*	IBTOTAL N BETWEEN V	*SUBTOTAL NORTH SIDE OF 14 STREET= BETWEEN WEST AVE AND ALTON ROAD	ET=	107	ις	0	0	75		6	0	N/A	N/A	0	0	N/A	N/A	N/A	N/A	0	0	N/A	N/A	
30	15 St	Between Bay Road and West Ave	South	45	N/A					3		21	1					-24	N/A					
DS	BTOTAL SA BETWEEN	SUBTOTAL SOUTH SIDE OF 15 STREET= BETWEEN BAY ROAD AND WEST AVE	=13	45	N/A	0	0	0	0	м	0	21	н	0	0	0	0	-24	N/A	0	0	0	0	
31	15 St	Between Bay Road and	North	49	N/A					2		0	0					-49	N/A					
32	15 St	Between Bay Road and West Ave	North	175	N/A					6		201	6					26	N/A					One Proposed Handicap Parking
S	BTOTAL N BETWEEN	SUBTOTAL NORTH SIDE OF 15 STREET= BETWEEN BAY ROAD AND WEST AVE	ET=	224	N/A	0	0	0	0	11	0	201	6	0	0	0	0	-23	N/A	0	0	0	0	
33	15 St	Between West Ave and Alton Road	South	39	2					п		49	2					10	0					One Proposed Handicap Parking
34	15 St	Between West Ave and Alton Road	South			10	2				S			0	0					-10	-5			Motorcycle
35	15 St	Between West Ave and Alton Road	South	19	1					1		0	0					-19	t.					
37	15 St	Between West Ave and Alton Road	South	21	1					1		0	0					-21	-		1			
38	15 St	Between West Ave and Alton Road	South	64	ю					1		42	2	1				-22	17					
SI	JBTOTAL S BETWEEN	SUBTOTAL SOUTH SIDE OF 15 STREET= BETWEEN WEST AVE AND ALTON ROAD	ET=	143	7	10	2	0	0	4	īU	91	4	0	0	0	0	-52	e,	-10	-5	0	0	
36	15 St	Between West Ave and Alton Road	North	118	9					9		93	4					-25	-2					One Proposed Handicap Parking
39	15 St	Between West Ave and Alton Road	North	119	9					m	1	108	w					-11	-1					
JS	JBTOTAL P BETWEEN	SUBTOTAL NORTH SIDE OF 15 STREET= BETWEEN WEST AVE AND ALTON ROAD	EET=	237	12	0	0	0	0	6	1	201	6	0	0	0	0	-36	ę.	0	0	0	0	
42	15 Ter	Between Bay Road and West Ave	South	64	N/A					8		23	1					-41	N/A					

Civil M	Civil Works, Inc						WES	T AVE	NOE	MPR	WEST AVENUE IMPROVEMENTS - NORTH PROJECT	ENTS	NO.	RTHF	ROJ	ECT.								May 10, 2020
***************************************							EXIS	EXISTING						PROPOSED	SED			decentarion of the second						
			-			Type of Parking	Parking							Type of Parking	rking				Net G	in/Loss of	Net Gain/Loss of Parking [LF]			
Location	Street	limits	Side	Parallel	ıllel	Perpendicular	dicular	Angle	gı.	Spaces Occupied	peidnoo	Parallel	lel	Perpendicular	cular	Angle	41	Parallel	_	Perpendicular	ular	Angle	a)	
Number				<u>"</u>	# of Marked Spaces	5	# of Marked Spaces	1	# of Marked Spaces	Cars	Motorcycle	77	# of Marked Spaces	7 8	# of Marked Spaces	5	# of Marked Spaces	프	# of Marked Spaces	∓ 5 8	# of Marked Spaces	5	# of Marked Spaces	Comments
43	15 Ter	Between Bay Road and West Ave	South	36	N/A					2		0	0					-36	N/A					
SUE	STOTAL SO BETWEEN	SUBTOTAL SOUTH SIDE OF 15 TERRACE= BETWEEN BAY ROAD AND WEST AVE	===	100	N/A	0	0	0	0	ις.	0	23	Ħ	0	0	0	0	<i>TT-</i>	N/A	0	0	0	0	
44	15 Ter	Between Bay Road and West Ave	North	17	N/A					1		0	0					-17	N/A					
45	15 Ter	Between Bay Road and West Ave	North	19	N/A					1		0	0					-19	N/A					
46	15 Ter	Between Bay Road and West Ave	North	97	N/A					2		21	1					-25	N/A					
SUE	STOTAL NC BETWEEN	SUBTOTAL NORTH SIDE OF 15 TERRACE= BETWEEN BAY ROAD AND WEST AVE	CE=	82	0	0	0	0	0	4	0	21	п	0	0	0	0	-61	N/A	0	0	0	0	
47	15 Ter	Between West Ave and Alton Road	South	100	r.					ın		93	4					-2	Ţ.					One Proposed Handicap Parking
48	15 Ter	Between West Ave and Alton Road	South	17	1					1		0	0					-27	ţ.					Handicap
80	15 Ter	Between West Ave and Alton Road	South	36	2					2		20	1					-16	-1					
51	15 Ter	Between West Ave and Alton Road	South			6	2				4			0	0					ę.	-5			Motorcycle Parking
SO	BTOTAL SC BETWEEN	SUBTOTAL SOUTH SIDE OF 1.5 TERRACE= BETWEEN WEST AVE AND ALTON ROAD	\CE=	163	∞	ō	2	0	0	œ	4	113	S	0	0	0	0	-50	ε'n	ο̈́	-5	0	0	
49	15 Ter	Between West Ave and Alton Road	North	711	9					9		93	4					-24	-5					One Proposed Handicap Parking
52	15 Ter	Between West Ave and Alton Road	North	20	н					1		0	0					-20	단					
53	15 Ter	Between West Ave and Alton Road	North	102	2					2		102	4					0	77					Freight Loading Zone
SO	BTOTAL NO BETWEEN	SUBTOTAL NORTH SIDE OF 15 TERRACE= BETWEEN WEST AVE AND ALTON ROAD	ACE=	139	12	0	0	0	0	12	0	195	60	0	0	0	0	44	4	0	0	0	0	
56	16 St	Between Biscayne Bay and Bay Road	South	N/A	N/A							N/A	N/A					N/A	N/A					Approx 182' of private parking. No changes
57	16 St	Between Biscayne Bay and Bay Road	North	N/A	N/A							N/A	N/A					N/A	N/A					Approx 102' of private parking. No changes
	SUB BETWEEN	SUBTOTAL 16 STREET= BETWEEN BISCAYNE BAY ROAD		N/A	N/A	18	4	0	0	N/A	0	N/A	N/A	0	0	0	0	N/A	N/A	0	0	0	0	Approx 284' of private parking. No changes
58	16 St	Between Bay Road and West Ave	South	149	N/A					7		115	S					-34	N/A					One Proposed Handicap Parking

Civil Wo	Civil Works. Inc		-				WEST	TAV	ENUE	IMPR	AVENUE IMPROVEMENTS - NORTH PROJECT	ENTS	S- NO	RTH	ROJ	ECT								May 10, 2020
						-	EXIS	EXISTING						PROPOSED	SED		H						П	
						Type of Parking	Parking				3			Type of Parking	arking				Net G	Net Gain/Loss of Parking [LF]	Parking [LF			
Location	Street		3.0	Parallel	lel	Perpen	Perpendicular	Angle	le le	Spaces (	Spaces Occupied	Parallel	lel	Perpendicular	icular	Angle	4	Parallel	F.	Perpendicular	ular	Angle	a.	
Number	Name	SI	appic	ш.	# of Marked Spaces	<u>"</u>	# of Marked Spaces	5	# of Marked Spaces	Cars	Motorcycle	11	# of Marked Spaces	- H	# of Marked Spaces	느	# of Marked Spaces	7 2 8	# of Marked Spaces	F S N	# of Marked Spaces	5	# of Marked Spaces	Comments
59	16 St	Between Bay Road and West Ave	South	13	N/A					Ħ		0	0					-13	N/A					
09	16 St	Between Bay Road and West Ave	South	25	N/A					1		0	0					-25	N/A					
SU	STOTAL SC BETWEEN	SUBTOTAL SOUTH SIDE OF 16 STREET= BETWEEN BAY ROAD AND WEST AVE	=E	187	N/A	0	0	0	0	6	0	115	Ŋ	0	0	0	0	-72	N/A	0	0	0	0	One Proposed Handicap Parking
61	16 St	Between Bay Road and	North	46	N/A					ю		115	5					69	N/A					One Proposed Handicap Parking
62	16 St	Between Bay Road and	North	30	N/A					2		0	0					-30	N/A	1				
63	16 St	Between Bay Road and West Ave	North	19	1					1	1	0	0					-19	r.					
INS	STOTAL N BETWEEN	SUBTOTAL NORTH SIDE OF 16 STREET=	= 13	95	N/A	0	0	0	0	9	1	115	rv	0	0	0	0	20	N/A	0	0	0	0	One Proposed Handicap Parking
64	16 St	Between West Ave and Alton Road	South	118	9					9		64	m					-54	ψ					
29	16 St	Between West Ave and Alton Road	South	58	N/A					0		62	m					4	N/A					Exisitng space used to have restaurant tables
ns	BTOTAL S	SUBTOTAL SOUTH SIDE OF 16 STREET= BETWEEN WEST AVE AND ALTON ROAD	= 13	176	N/A	0	0	0	0	9	0	126	9	0	0	0	0	-50	N/A	0	0	0	0	58 LF of exisitng space used to have restaurant tables
65	16 St	Between West Ave and Alton Road	North	119	N/A					9	1	96	4					-23	N/A					One Proposed Handicap Parking
99	16 St	Between West Ave and Alton Road	North	39	N/A					2	1	22	Н					-17	N/A					
DS	BTOTAL N BETWEEN	SUBTOTAL NORTH SIDE OF 16 STREET= BETWEEN WEST AVE AND ALTON ROAD	ET=	158	N/A	0	0	0	0	00	2	118	S	0	0	0	0	-40	N/A	0	0	0	0	
88	Bay Road	Between Collins Canal and Lincoln Rd	East	48	N/A					2		20						-28	N/A					
88	Bay Road	Betwee	East	20	N/A						4	0	0					-20	N/A					
06	Bay Road	Betwee	East	55	N/A					3		16	1					-39	N/A					
91	Bay Road	Between	East	44	N/A					ю	ч	20	1					-24	N/A					
92	Bay Road	Between Collins Canal and Lincoln Rd	East	37	N/A					2		22	1					-15	N/A					
93	Bay Road	Between Collins Canal and Lincoln Rd	East	18	N/A					1		0	0					-18	N/A					
S	UBTOTAL	SUBTOTAL EAST SIDE OF BAY ROAD= BETWEEN COLLINS CANAL AND LINCOLN ROAD	AD AD	222	N/A	0	0	0	0	Я	ru	78	4	0	0	N/A	N/A	-144	N/A	0	0	0	0	
Spaces Occ	upied: Refers	Spaces Occupied: Refers to actual vehicles parked on Thursday 04-14-2022 from 8:00 PM to 11:00 PM.	nursday 04-1.	4-2022 fron	n 8:00 PM t	:0 11:00 PN	4																	17102 00 and 17105.0

Side	Ang	Spaces Occupied  Cars Motorcycle	Para	PROPOSED  Type of Parking Perpendicular	ED				Gain/Loss	Net Gain/Loss of Parking [LF]	1		
Street         Limits         Side         Parallel         Perpendicular         Angle         Spaces         Angle         Angle <td>Ang</td> <td>s s</td> <td>Para</td> <td>Type of Parl Perpendicu</td> <td>ding</td> <td></td> <td></td> <td></td> <td>Gain/Loss</td> <td>of Parking [</td> <td>IH.</td> <td></td> <td></td>	Ang	s s	Para	Type of Parl Perpendicu	ding				Gain/Loss	of Parking [	IH.		
Street         Limits         Side         Parallel         Perpendicular         Angle         Januarie           Bay Road         Between Collins Canal and Lincoln Rd         West         28         1         # of Ansked         LF Marked Lincoln Rd         # of Ansked Canal and Rd         West         28         1         Anaked Canal and Lincoln Rd         Respaces         1         N/A         N/A         N/A         1         1           Bay Road         Between Collins Canal and Lincoln Rd         West         17         N/A         N/A         N/A         N/A         N/A         1           Bay Road         Between Collins Canal and Lincoln Rd         West         17         N/A         N/A         N/A         1         1           Bay Road         Between Collins Canal and Lincoln Rd         West         17         N/A         N/A         N/A         N/A         1         1           Bay Road         Between Collins Canal and Lincoln Rd         West         17         N/A	Ang		Para	Perpendicu		STATISTICS OF THE PERSON OF TH	Property of the Party of the Pa			CONTRACTOR CONTRACTOR CONTRACTOR	-		
Bay Road         Between Collins Canal and Lincoln Rd         West         24         N/A         # of Marked Lincoln Rd         # of Marked Lincoln Rd         # of Spaces         #	5		H		lar	Angle		Parallel	Perper	Perpendicular	An	Angle	
Bay Road   Between Collins Canal and   West   24   N/A		0	LF Marked Spaces	LF Sp	# of Marked L Spaces	# of LF Marked Spaces	sd LF	# of Marked Spaces	5	# of Marked Spaces	5	# of Marked Spaces	Comments
Bay Road   Between Collins Canal and Lincoln Rd   West   24 N/A   N/A			0 0				-28	4					Handicap
Bay Road   Between Collins Canal and Lincoln Rd   Bay Road   Between Collins Canal and Lincoln Rd   Bay Road   Between Collins Canal and Lincoln Rd   Bay Road   Between Collins Canal and Rest   17   N/A		1	22 1				-2	N/A					Existing Commercial Loading Zone
Bay Road   Between Collins Canal and   West   17   N/A   N/A		2	0				-30	N/A					
Bay Road   Between Collins Canal and   West   75   N/A		1 1	0 0				-17	N/A					
Bay Road   Between Collins Canal and   West   17   N/A   N		3	44 2				-31	N/A					
Bay Road   Between Collins Canal and Lincoln Rd   West   24 N/A   N/A   Details   Collins Canal and Lincoln Rd   Lincoln Rd   Lincoln Rd and Lincoln Rd and Lincoln Rd and Raw Road   Between Uncoln Rd and Raw Road   Lincoln Rd and		1	0 0				-17	N/A					
SUBTOTAL WEST SIDE OF BAY ROAD=         215         N/A         0		1	0 0				-24	N/A					
Bay Road Between Uncoln Rd and East 27 1  Raw Road Between Uncoln Rd and Fast 24 N/A	0	9 1	99	0	0	0 0	-149	N/A	0	0	0	0	
Bay Road Between Lincoln Rd and Fast 24 N/A		1	22 1				τŲ	0					Existing Handicap
Lincoln Ter		1	0 0				-24	N/A					
103 Bay Road Etween Lincoln Rd and East 24 N/A 7		7	28 1				4	N/A					One Proposed Handicap Space
104 Bay Road Between Lincoln Rd and East 26 N/A 1		1	0 0				-26	N/A					
SUBTOTAL EAST SIDE OF BAY ROAD=  BETWEEN LINCOLN ROAD AND LINCOLN TERRACE  101  N/A  0  0  0  10  10	0	10 0	50 2	0	0	0	-51	N/A	0	0	0	0	
105 Bay Road Between Lincoln Rd and West 37 2 3		E.	0 0				-37	-5					
106 Bay Road Between Lincoln Rd and West 80 4 4		4	99				-14	4					
SUBTOTAL WEST SIDE OF BAY ROAD=  BETWEEN LINCOLN ROAD AND LINCOLN TERRACE  117 6 0 0 0 7  PETWEEN LINCOLN BOAD AND LINCOLN TERRACE	0	7 0	99	0	0	0 0	-51	m	0	0	0	0	
107         Bay Road         Between Lincoln Ter and 16         East         46         2         2         2		2	22 1				-24	ų					
108         Bay Road         Between Lincoln Ter and 16         West         60         3         3		ю	0 0				09-	ę					
SUBTOTAL SIDE OF BAY ROAD= 106 5 0 0 0 0 5 S BETWEEN LINCOLIN TERRACE AND 16 STREET	0	0	22 1	0	0	0 0	-84	4	0	0	0	0	
109 Bay Road Between 16 St and 15 Ter East 46 N/A 2		2	42 2				4	N/A					

5 of 9

Civil Wo	Civil Works, Inc	magnetical contribution of the contribution of					WES.	T AVE	NUE	MPR	WEST AVENUE IMPROVEMENTS			THP	- NORTH PROJECT	5	-						May 10, 2020
							EXISTING	.ING						PROPOSED	G		1					-	
			1	-		Type of Parking	arking							Type of Parking	king				Net Gain/	Net Gain/Loss of Parking [LF]	ng [LF]		
Location	Street	imite	- Sign	Parallel	lel	Perpendicular	icular	Angle		Spaces Occupied	cupied	Parallel	  -	Perpendicular	ılar	Angle		Parallel	Pe	Perpendicular		Angle	Comments
Number	Name	2		5	# of Marked Spaces	5	# of Marked Spaces	LF	# of Marked C Spaces	Cars Mc	Motorcycle	7 8	# of Marked Spaces	LF NR	# of Marked Spaces	# LF Ma	# of Marked LF Spaces	# of Marked Spaces	ed LF	# of Marked Spaces	A LE	# of Marked Spaces	
110	Bay Road	Between 16 St and 15 Ter	East	20	N/A					1		20	1			1	0	N/A	A		-		
SS	BTOTAL E	SUBTOTAL EAST SIDE OF BAY ROAD= BETWEEN 16 STREET AND 15 TERRACE	ш	99	N/A	0	0	0	0	ю	0	62	ю	0	0	0	4	t N/A	0	0	0	0	
111	Bay Road	Between 16 St and 15 Ter	West	109	N/A					ın	1	44	2			1	-65	S N/A	₫				
SS	BTOTAL W BETWEEN	SUBTOTAL WEST SIDE OF BAY ROAD= BETWEEN 16 STREET AND 15 TERRACE	<u>"</u>	109	N/A	0	0	0	0	rv.	1	44	2	0	0	0	0 -65	S N/A	0	0	0	0	
112	Bay Road	Between 15 Ter and 15 St	East	43	N/A					2		42	2			$\dashv$	4	1 N/A	A	-	1		
113	Bay Road	Between 15 Ter and 15 St	East	63	N/A					Э	1	44	2				-19	9/N	A				
าร	BETWEEN	SUBTOTAL EAST SIDE OF BAY ROAD= BETWEEN 15 TERRACE AND 15 STREET	ı,	106	N/A	0	0	0	0	ru	-	98	4	0	0	0	020	00 N/A	0	0	0	0	
114	Bay Road	Between 15 Ter and 15 St	West	32	N/A					1	ж	88	4				26	6 N/A	A				
115	Bay Road	Between 15 Ter and 15 St	West	25	п					1	1	23	7		+		· ·	-2 0	_				
116	Bay Road	Between 15 Ter and 15 St	West	70	1					п		0	0				7.7	-20 -1	1				Taxí Zone
117	Bay Road	+	West	36	N/A					2		22	1				7	-14 N/	N/A				One Proposed Handicap Space
જ	JBTOTAL V BETWEEN	SUBTOTAL WEST SIDE OF BAY ROAD= BETWEEN 15 TERRACE AND 15 STREET		113	N/A	0	0	0	0	N	4	133	٥	0	0	0	0 2	20 N/	N/A 0	0	0	0	
118	Bay Road	Between 15 St and Flamingo Way	East	44	N/A					2	Ħ	44	2					Ž O	N/A				
119	Bay Road	Between 15 St and Flamingo Way	East	29	N/A					2		22	п					, V	N/A				
S	UBTOTAL BETWEEN 1	SUBTOTAL EAST SIDE OF BAY ROAD= BETWEEN 15 STREET AND FLAMINGO WAY	<b>=</b> .	73	N/A	0	0	0	0	4	П	99	m	0	0	0	0	Y .	N/A	0	0	0	ora Danded Handing
120	Bay Road	Between 15 St and Flamingo Way	West	112	N/A					9		157	7					45 N	N/A		-		Space
SS	JBTOTAL 1	SUBTOTAL WEST SIDE OF BAY ROAD= BETWEEN 15 STREET AND FLAMINGO WAY	, ,	112	N/A	0	0	0	0	9	0	157	7	0	0	0	0	N N	N/A	0	0	0	
121	Bay Road	Between Flamingo Way and	East	20	N/A					1		20	1					0	N/A		-	1	
122	Bay Road	Between F	East	45	N/A					2		44	2		+			1- N	N/A		-		
S	UBTOTAL BETWEEN FL	SUBTOTAL EAST SIDE OF BAY ROAD= BETWEEN ELAMINGO WAY AND 14 TERRACE	— <u>О</u> =	65	N/A	0	0	0	0	m	0	64	m	0	0	0	0	-1- N	N/A	0 0	0	0	
					00.0	11.00 PM																	

No.   No.	Civil M	Civil Works, Inc						WEST A		ENDE	IMPE	<b>VENUE IMPROVEMENTS</b>	ENT	NO - S	RTH	- NORTH PROJECT	ECT							İ	May 10, 2020
Part	warpionis-ottoppintos						-	EXIS	TING						PROPC	SED		H							
Part							Type o	f Parking							Type of P	arking				Net G	nin/Loss of	Parking [LF	-		
Part	Location		1	Cido	P.	rallel	Perpe	ndicular	Ang	şle	Spaces	Occupied	Para	lel	Perpend	icular	Angl	<u>е</u>	Parall	- A	Perpendic	ular	Angl	a	
	Number			200	5	# of Marked Spaces		# of Marked Spaces	5	# of Marked Spaces	Cars	Motorcycle	I.F	# of Marked Spaces		# of Marked Spaces		# of Marked Spaces		# of Aarked ipaces		# of 1arked ipaces		# of Marked Spaces	Comments
	123	Bay Road	_	West	88	N/A					4		99	8					-23	N/A					
Health   H	124	Bay Road	-	West	24	N/A					1		0	0					-24	N/A					
Supportal   Suppose   Street	3	JBTOTAL \ BETWEEN FL	WEST SIDE OF BAY ROA	(D=	113	N/A	0	0	0	o	ī.	0	99	es	0	0	0	0	47	N/A	0	0	0	0	
	125	Bay Road	Between 14 Ter and 14 St	1	23	N/A					0		0	0					-23	N/A					Passenger Loading Zone
Notice   March   Mar	126	Bay Road	+	East	22	N/A					1		44	2					22	N/A					
Figure   Particle	S	UBTOTAL	EAST SIDE OF BAY ROAN 14 TERRACE AND 14 STREET	ä	45	N/A	0	0	0	0	1	0	44	2	0	0	0	0	1-	N/A	0	0	0	0	
Simple   S	127	Bay Road	-	West	63	N/A			30		2		99	т					т	N/A					
37         NAA         0         0         0         0         0         0         0         10         0 <td>128</td> <td>Bay Road</td> <td>+</td> <td>West</td> <td>46</td> <td>N/A</td> <td></td> <td></td> <td></td> <td></td> <td>2</td> <td></td> <td>44</td> <td>2</td> <td></td> <td></td> <td></td> <td></td> <td>-7</td> <td>N/A</td> <td></td> <td></td> <td></td> <td>1</td> <td></td>	128	Bay Road	+	West	46	N/A					2		44	2					-7	N/A				1	
37         N/A         0	<u>v</u>	UBTOTAL	WEST SIDE OF BAY ROAN 14 TERRACE AND 14 STREET	=Q\ =	109	N/A	0	0	0	0	4	0	110	rv	0	0	0	0		N/A	0	0	0	0	
4         4	26	Flamingo		North	37	N/A					н		0	0					-37	N/A					
4         4		SUBTO	TAL FLAMINGO WAY=		37	N/A	0	0	0	0	1	0	D	0	0	0	0	0	-37	N/A	0	0	0	0	
1         1	89	Lincoln Ra	_				18	1			1				0	0					-18	ţ.			Handicap
4         5         6         7         9         21         1         9         0	69	Lincoln Ro	-				53	9			9		21	П							-32	rγ			
44         2         60         7         90         7         90 <td>SUB</td> <td>TOTAL SO</td> <td>NUTH SIDE OF LINCOLN F</td> <td>ROAD=</td> <td>0</td> <td>0</td> <td>71</td> <td>7</td> <td>0</td> <td>0</td> <td>7</td> <td>0</td> <td>21</td> <td>п</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>-50</td> <td>φ</td> <td>0</td> <td>0</td> <td></td>	SUB	TOTAL SO	NUTH SIDE OF LINCOLN F	ROAD=	0	0	71	7	0	0	7	0	21	п	0	0	0	0	0	0	-50	φ	0	0	
44         2         6         6         7         9         7         9         9         7         9         9         7         9	70	Lincoln Rc	_				09	7			7						24	2	1		-36	rὑ			
44         2         0	SUB	TOTAL NO BETWEEN BE	ORTH SIDE OF LINCOLN I	ROAD=	0	0	09	7	0	0	7	0	0	0	0	0	24	2	0	0	-36	ν̈́	0	0	
44         2         0	71	Lincoln Rt			44	2					2		21	1					-23	다					
Lincoln Rd         Between Lincoln Ct and Bay Rd         North Rd         So         3         4         9         0         0         0         0         -50         -3           Lincoln Rd         Between Lincoln Ct and Bay Rd         North         9         1         0         0         0         0         -9         -1         1         -9         -1 <t< td=""><td>sub</td><td>TOTAL SO BETWEEN</td><td>OUTH SIDE OF LINCOLN I</td><td>ROAD=</td><td>44</td><td>2</td><td>0</td><td>0</td><td>0</td><td>0</td><td>2</td><td>0</td><td>21</td><td>1</td><td>0</td><td>0</td><td>0</td><td>0</td><td>-23</td><td>4</td><td>0</td><td>0</td><td>0</td><td>0</td><td></td></t<>	sub	TOTAL SO BETWEEN	OUTH SIDE OF LINCOLN I	ROAD=	44	2	0	0	0	0	2	0	21	1	0	0	0	0	-23	4	0	0	0	0	
Lincoln Rd         Between Lincoln Ct and Bay         North         9         1         0         0         0         0         -1         -9         -1           Lincoln Rd         Between Lincoln Ct and Bay         North         38         4         3         4         3         44         3	72	Lincoln Ru							20	е	4						0	0					-50	ņ	
Lincoln Rd         Between Lincoln Rd         82         7         44	73	Lincoln R							6	п	0						0	0	$\dagger$				6-	7	Passenger Loading Zone
	74	Lincoln R			$\Box$				38	4	8						82	7			1	1	44	е	

Particular   Par	Civil IM	Civil Works Inc						WEST A	T AVE	NUE	MPR	VENUE IMPROVEMENTS	ENTS	N-	- NORTH PROJECT	ROJ	ECT								May 10, 2020
No.	TAIS .	ours, and		F		Manageory Salaman Company		EXIS	ING						PROPO	SED								П	
Parish   P				1_			Type of F	arking				-			Type of Pa	ırking				Net Ga	in/Loss of	Parking (LF,			
Part	Location		49.00	- Gio	Parall	lel	Perpend	licular	Angle		Spaces O	peidnoo	Para	lel	Perpendi	cular	Angle		Parall	To 1	Perpendic	ular	Angle	đi.	
	Number			anic .		# of Marked Spaces	7	# of Marked Spaces		# of Marked Spaces		Motorcycle		# of Marked Spaces		# of Marked Spaces		# of Aarked spaces		# of Aarked Spaces		# of flarked spaces		# of Marked Spaces	Comments
	SUBT	OTAL NOR	TH SIDE OF LINCOLN RACOLN COURT AND BAY ROAD	DAD=	1	0	0	0	97	80	7	0	0	0	0	0	82	7	0	0	0	0	-15	-1	One Proposed Handicap Parking
31         32<	75	Lincoln Rd	Between Bay Road and						137	7	7						103	80					-34		Proposed Spaces include one hadicap parking
131         31	92	Lincoln Rd	Between Bay Road and Wast Ave	South	39	2					8						96	6	57	7		1			
48         5         6         7         6         6         6         7         139         1	77	Lincoln Rd		South	59	8					2		0	0					-59	e,				1	Freight Loading Zone
137         13         14	SUBT	OTAL SOU	TH SIDE OF LINCOLN R BAY ROAD AND WEST AVE	OAD=	88	Ŋ	0	0	137	7	12	0	0	0	0	0	199	17	-2	4	0	0	-34	-	One Proposed Handicap Parking
137         2         1         2         2         1         2         2         3         4         3         4	78	Lincoln Rd	Between Bay Road and West Ave	North					66	œ	00						72	9					-27	-2	
137         3         4	79	Lincoln Rd	_	North	137	2					2		0	0					-137	-2			1		
15.         15. <td>SUBT</td> <td>TOTAL NOR</td> <td>TH SIDE OF LINCOLN R BAY ROAD AND WEST AVE</td> <td>OAD=</td> <td>137</td> <td>2</td> <td>0</td> <td>0</td> <td>66</td> <td>00</td> <td>10</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>72</td> <td>9</td> <td>-137</td> <td>-2</td> <td>0</td> <td>0</td> <td>-27</td> <td>-5</td> <td></td>	SUBT	TOTAL NOR	TH SIDE OF LINCOLN R BAY ROAD AND WEST AVE	OAD=	137	2	0	0	66	00	10	0	0	0	0	0	72	9	-137	-2	0	0	-27	-5	
47         27         37         47<	8	Lincoln Rd		South	15	8						ю	0	0					-15	ę.					Motorcycle Parking
47         2         4	81	Lincoln Rd	-	South	22	н					1		0	0					-22	7					
15.1         3.1         3.1         4.1         3.1         4.1         4.1         4.1         6.1         6.1         6.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5         7.5 <td>83</td> <td>Lincoln Rd</td> <td>-</td> <td>South</td> <td>47</td> <td>2</td> <td></td> <td></td> <td></td> <td></td> <td>2</td> <td></td> <td>84</td> <td>4</td> <td></td> <td></td> <td></td> <td></td> <td>37</td> <td>2</td> <td></td> <td></td> <td></td> <td></td> <td></td>	83	Lincoln Rd	-	South	47	2					2		84	4					37	2					
151         9         0         0         0         9	84	Lincoln Rd	<u> </u>	South	67	е					п		0	0					-67	ę					Taxi Zone
4         4	SUB	TOTAL SOU	JTH SIDE OF LINCOLN F WEST AVE AND ALTON ROAD	OAD=	151	6	0	0	0	0	4	æ	84	4	0	0	0	0	19-	4	0	0	0	0	SS are used to nave
4.5         1.5 <td>82</td> <td>Lincoln Rd</td> <td></td> <td>North</td> <td></td> <td></td> <td></td> <td></td> <td>97</td> <td>7</td> <td>ж</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>91</td> <td>7</td> <td></td> <td></td> <td></td> <td></td> <td>φ</td> <td></td> <td>restaurant tables (4 Spaces). Proposed Spaces include one</td>	82	Lincoln Rd		North					97	7	ж						91	7					φ		restaurant tables (4 Spaces). Proposed Spaces include one
21         N/A         N/A         20         0         0         0         0         0         187         187         15         0         0         -15         0         -15           242         N/A         N/A <td>88</td> <td>Lincoln Rd</td> <td></td> <td>North</td> <td></td> <td></td> <td></td> <td></td> <td>105</td> <td>∞</td> <td>7</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td>96</td> <td>00</td> <td></td> <td></td> <td></td> <td></td> <td>6</td> <td>0</td> <td>14' are used to have restaurant tables (1 Space).</td>	88	Lincoln Rd		North					105	∞	7						96	00					6	0	14' are used to have restaurant tables (1 Space).
Lincoin Terror   Between Biscavire Bay and South   S	SUB	TOTAL NOF	RTH SIDE OF LINCOLN F	tOAD=	0	0	0	0	202	15	10	0	0	0	0	0	187	15	0	0	0	0	-15	0	
Lincoln Tex   Between Biscavine Bay and   North   242   N/A   North   242   N/A   North   242   N/A   North   242   N/A   North   No	98	Lincoln Ter		South	221	N/A					12		509	0					-12	N/A		1			
SUBTOTALLINCOLNTERRACE=         463         N/A         0         0         0         438         0	87	Lincoln Ter	-	North	242	N/A					7		229	0					-13	N/A					
West Ave         Between 14St and 14Ter         West         23         N/A         0         0         0         0         -23           West Ave         Between 14St and 14Ter         West         19         N/A         0         0         0         0         -19		SUBTOT, BETWEEN	'AL LINCOLN TERRACE= BISCAYNE BAY AND BAYROAD		463	N/A	0	0	0	0	19	0	438	0	0	0	0	0	-25	N/A	0	0	0	0	
West Ave         Between 14 St and 14 Ter         West         19         N/A         0         0         0         0         -19	14	West Ave		West	23	N/A					0		0	0					-23	N/A					
	15	West Ave		West	19	N/A					0		0	0					-19	N/A		$\dashv$			

Civil M	Civil Works, Inc						WEST A	T AVI	NOE	IMPR	VENUE IMPROVEMENTS	ENTS	- NO	- NORTH PROJECT	ROJE	5							May 10, 2020	
- Contract of the Contract of				-			EXIS	EXISTING						PROPOSED	SED									T
***************************************						Type of	Type of Parking							Type of Parking	rking				Net Gai	Net Gain/Loss of Parking [LF]	rking [LF]		T	
Location	Street	limit	S. P.	Par	Parallel	Perpen	Perpendicular	Angle	a	Spaces Occupied	peidnoo	Parallel	lel	Perpendicular	ular	Angle		Parallel		Perpendicular	ar	Angle		
Number		1		5	# of Marked Spaces	7	# of Marked Spaces	5	# of Marked Spaces	Cars	Motorcycle	5	# of Marked Spaces	LF N	# of Marked Spaces	LF M	# of Marked Spaces	LF Ma	# of Marked Spaces	# LF Ma Spa	# of Marked L Spaces	# of LF Marked Spaces	f Comments ed	
16	West Ave	Between 14 Stand 14 Ter	West	25	N/A					1		0	0					-25 N	N/A					
S	UBTOTAL E	SUBTOTAL EAST SIDE OF WEST AVE BETWEEN 14 STREET AND 14 TERRACE	<u></u>	19	N/A	0	0	0	0	н	0	0	0	0	0	0	0	-67 P	N/A	0	0	0		
17	West Ave	Between 14 St and 14 Ter	East	22	N/A					1	1	0	0					-22 P	N/A					
18	West Ave	Between 14 St and 14 Ter	East	103	N/A					Ω.	2	88	4					-15 N	N/A		-			
S	UBTOTAL B BETWEEN	SUBTOTAL EAST SIDE OF WEST AVE= BETWEEN 14 STREET AND 14 TERRACE	Ш	125	N/A	0	0	0	0	Ø	т	88	4	0	0	0	0	-37	N/A	0	0	0		
13	West Ave	Between 14 Ter and Flamingo Way	West	63	N/A					ю		0	0					-63	N/A		-	-		
S	UBTOTAL V	SUBTOTAL WEST SIDE OF WEST AVEBETWEEN 14 TERRACE AND FLAMINGO WAY	Ħ.	63	N/A	0	0	0	0	m	0	0	0	0	0	0	0	-63	N/A	0	0	0		
27	West Ave	Between Flamingo Way and 15 St	West	7.7	1					1		0	0					-27	단		-		Handicap	
28	West Ave	Between Fl	West	136	N/A					9		0	0					-136	N/A					
S	UBTOTAL \ BETWEEN FL	SUBTOTAL WEST SIDE OF WEST AVE BETWEEN FLAMINGO WAY AND 15 STREET	£ .	163	N/A	0	0	0	0	7	0	0	0	0	0	0	0	-163	N/A	0	0	0		
29	West Ave	Between Flamingo Way and 15 St	East	151	N/A					œ		147	7					4	N/A					
J,	SUBTOTAL BETWEEN FL	SUBTOTAL EAST SIDE OF WEST AVE BETWEEN FLAMINGO WAY AND 15 STREET	<u>ш</u> .	151	N/A	0	0	0	0	60	0	147	7	0	0	0	0	4	N/A	0	0	0		
40	West Ave	Between 15 St and 15 Ter	West	181	N/A					6		0	0					-181	N/A					
41	West Ave	Between 15 St and 15 Ter	East	139	N/A					9	3	128	9		1			11-	N/A		1	+		T
	SUB	SUBTOTAL WEST AVE= BETWEEN 15 STREET AND 15 TERRACE		320	N/A	O	0	0	0	15	ю	128	9	0	0	0	0	-192	N/A	0	0	0	0	
54	West Ave	Between 15 Ter and 16 St	East	63	N/A					3		22	1					-41	N/A					
55	West Ave	Between 15 Ter and 16 St	West	55	N/A					2		0	0					-55	N/A					
	SUB	SUBTOTAL WEST AVE= BETWEEN 15 TERRACE AND 16 STREET		118	N/A	0	0	0	0	S	0	22	1	0	0	0	0	96-	N/A	0	0		0	
								-1									+	-	_			-		
	TOT	TOTAL NORTH PROJECT		5955	N/A	171	20	535	38	341	35	3755	147	0	0	564	47	-2125	N/A	-126	0	-91	0	

11	Croit Works, Inc.   Cocation   Street   Limits   Side   FI   Marked   FI   Marked	Perpendicular   Side	NEST A	NEST A	WEST A	A TSIING F	A TSIING F		# of # of Marked Spaces	<u> </u>	Spaces Cars	MPROVEM Spaces Occupied	Parallel FT Ma 5p	# Warl	PROPOSED Type of Parking Type of Parking Perpendicular Ang fed FI Marked FT Spaces	PROJ		# of Marked Spaces	Parallel # # FT Ma	g x s   4	ain/Loss of Parki Perpendicular # of FT Marke	Net Gain/Loss of Parking [LF] Perpendicular # of # of Marked ss Spaces	Ang	# of arked paces	May Tu, 2020 Comments Comments One Handicap Proposed Space
1.	00	15.80	Alton road Between West Ave and Alton Road	South	76	1					ın		88	4					φ	е п					
Substitutivity   Subs	1	8 St BTOTAL BETWEEN W	Between West Ave and Alton Road SOUTH SIDE OF 8 ST FET AVE AND ALTON ROAD	South	20 203	7 7	0	0	0	0	10	0	0 117	0 10	0	0	0	0	-20	- NA	0	0	0		Only 2 spaces are currently marked
State   Stat	10	8 St	Between West Ave and Alton Road	North	115	N/A					r.		73	ю					-42	N/A				0 0,	One Handicap Proposed Space
State   Stat	11	8 St	Between West Ave and Alton Road	North	118	ю					9		88	4			4		-30	н					
Signature Wist Advantage and Morth	S	JBTOTAL BETWEEN N	NORTH SIDE OF 8 S'	<u>"</u>	233	m	0	0	0	0	11	0	161	7	0	0	0	0	-72	N/A	0	0	0		Only 3 spaces are currently marked
9 st   Between West Ake and North   3st   2st    15	9 St	Between West Ave and Alton Road	North	27	T.					н		30	H					м	0					Handicap	
9 st   Retween West Avea and North Road	16	) S 6	Between West Ave and Alton Road	North			10	2	0			н			0	0					-10	-2			Motorcycle
9 st   Petty Rem West Ave and   North Road Alton Road Alton Road   North Road   N	17	9 St	Between West Ave and Alton Road	North	85	ю					2		44	2					-14	-1					
State   Between West Ave and Alton Road   North   G3   State   State	18	9 St	Between West Ave and Alton Road	North		le .	10	7	0		0				0	0					-10	-5		_	Motorcycle
Subtrotation North SIDE OF 9 table   18   24   26   46   26   47   26   47   26   47   26   47   26   47   26   47   26   47   26   27   27   27   27   27   27   2	13	9 St	Between West Ave and Alton Road	North	83	8					ю		63	е					0	0					
95t   Setween West Ave and Alton Road   South   20   1   2   2   3   3   3   3   3   3   3   3	Ñ	JBTOTAL BETWEEN V	NORTH SIDE OF 9 S WEST AVE AND ALTON ROAD	=	148	7	20	4	0	0	9	п	137	و	0	0	0	0	ti-	7	-20	4	0		1 Handicap and 4 existing Motorcycle spaces
95t   Settlemen West Ave and Alton Road   South   120   5   95t   State   105t   State   Sta	20	9 St	Between West Ave and Alton Road	South	30	н					0		0	0					-20	÷.					Passenger Loading Zone
SUBTOTAL SOUTH SIDE OF 9T=         140         6         0         0         0         83         3         0	21	9 St	Between West Ave and Alton Road	South	120	Ŋ					r,		83	ю					-37	-5					
10 St         Between West Ave and Alton Road         North         45         4         4         4         N/A         N/A	S	UBTOTAL BETWEEN V	SOUTH SIDE OF 9 S	=	140	9	0	0	0	0	5	0	83	m	0	0	0	0	-57	φ	0	0	0		Existing Passenger Loading Zone
	72	10 St	Between West Ave and Alton Road	North					45	4	4						N/A	N/A					N/A	N/A	

Spaces Occupied: Refers to actual vehicles parked on Thursday 04-14-2022 from 8:00 PM to 11:00 PM. \* Not included on the total

Civil Works, Inc	orks, Inc						WEST A		NOE	IMPR	VENUE IMPROVEMENTS - SOUTH PROJECT	ENTS	- 801	<b>ЛНР</b>	ROJE	CT CT								May 10, 2020
				-			EXIS	EXISTING						PROPOSED	ED									
						Type of Parking	arking							Type of Parking	king				Net Ga	Net Gain/Loss of Parking [LF]	arking [LF]			
ļ				Parallel	lei	Perpendicular	licular	Angle		Spaces Occupied	ccupied	Parallel	le le	Perpendicular	ılar	Angle		Parallel		Perpendicular	ular	Angle	e	
Number	Street Name	Limits	Side	E	# of Marked Spaces	E	# of Marked Spaces	E E	# of Marked Spaces	Cars	Motorcycle	E	# of Marked Spaces	FT SI	# of Marked Spaces	FT Mg	# of Marked Spaces	FT NR	# of Marked Spaces	FT S	# of Marked Spaces	Ē	# of Marked Spaces	Comments
28	10 St	Between West Ave and Alton Road	North					11	2		2					N/A	N/A					N/A	N/A	Motorcycle
29	10 St	Between West Ave and Alton Road	North					24	4	0						N/A	N/A					N/A	N/A	
15*	JBTOTAL BETWEEN W	*SUBTOTAL SOUTH SIDE OF 10 ST=	Ľ.	0	0	0	0	08	10	4	2	0	0	0	0	N/A	N/A	0	0	0	0	N/A	N/A	There is no proposed work on this area
30	10 St	Between West Ave and Alton Road	South					12	2		2					N/A	N/A					N/A	N/A	Motorcycle
31	10 St	Between West Ave and Alton Road	South					15	1	1						N/A	N/A					N/A	N/A	Handicap
32	10 St	Between West Ave and Alton Road	South			28	4				11			N/A	N/A					N/A	N/A			Motorcycle
33	10 St	Between West Ave and Alton Road	South	61	ю					е		N/A	N/A					N/A	N/A					
<b>IS</b> *	UBTOTAL BETWEEN V	*SUBTOTAL SOUTH SIDE OF 10 ST BETWEEN WEST AVE AND ALTON ROAD	<u> </u>	61	ю	28	4	72	ю	4	16	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	There is no proposed work on this area
34	10 St	Between Biscayne Bay and West Ave	North			33	4			4				0	0					-33	4			
35	10 St	Between Biscayne Bay and West Ave	North			15	2			2				0	0					-15	-5			
SL	JBTOTAL	SUBTOTAL NORTH SIDE OF 10 ST= Between Biscayne Bay and West Ave	Ľ.	0	0	48	9	0	0	9	0	0	0	0	0	0	0	0	0	-48	φ	0	0	
36	10 St	Between Biscayne Bay and West Ave	South			7.1	∞			7				0	0					-71	ερ			
S	JBTOTAL Between	SUBTOTAL SOUTH SIDE OF 10 ST= Between Biscayne Bay and West Ave	Ľ	0	0	71	∞	0	0	7	0	0	0	0	0	0	0	0	0	-71	×ρ	0	0	
39	11 St	Between West Ave and Alton Rd	South	26	1					0		34	1						0					Handicap
40	11 St	Between West Ave and Alton Rd	South	89	N/A					2		99	е П					-5	N/A					
41	11 St	Between West Ave and Alton Rd	South	72	1					н		0	0					-27	7		+			Handicap
42	11 St	Between West Ave and Alton Rd	South	55	2					н		0	0					-64	-2					Commercial Loading Zone
15	UBTOTAL	SUBTOTAL SOUTH SIDE OF 11 ST= Between West Ave and Alton Road	<u>"</u>	185	4	0	0	0	0	4	0	100	4	0	0	0	0	-85	N/A	0	0	0	0	Only 4 spaces are currently marked
43	11 St	Between West Ave and Alton Rd	North	22	1					0		34	1					7	0					Handicap
44	115t	Between West Ave and Alton Rd	North	77	17					0		22	1					1	0					

Maintain that the province of the province o	Wo	Civil Works, Inc						WES	WEST AV	ENUE	IMPE	/ENUE IMPROVEMENTS	ENTS		- SOUTH PROJECT	PRO	ECT								May 10, 2020
Part		-						EXI	STING						PROPO	SED		1	-						
							Type of	Parking			3	polining			Type of Pa	arking		1		Net G	ain/Loss of	Parking (LF			
Hand	-				Para	llel	Perpen	dicular	Ang	şle	Spaces	Occupied	Para	llel	Perpendi	cular	Angi	e e	Parall	-e	Perpendi	cular	Angl	9	
		Street	Limits	Side	E	# of Marked Spaces	E	# of Marked Spaces	E	# of Marked Spaces	Cars	Motorcycle		# of Marked Spaces		# of Marked Spaces		# of Marked Spaces		# of Marked Spaces		# of Viarked Spaces		# of Marked Spaces	Comments
	+	11 St	Between West Ave and Alton Rd	North	76	1					1		22	1					4	0					
Hand	-	11 St	Between West Ave and Alton Rd	North			7	2				0			0	0					7-	-5		2	Motorcycle
cuth         13         c         13         c         13         c         13         c         13         c	15	STOTAL N Between W	VORTH SIDE OF 11 ST Vest Ave and Alton Road	<u>n</u>	74	m	7	2	0	0	₽	0	78	т	0	0	0	0	4	0	-1	-5	0	0	
vind         11         1	Name of Street	12 St	Between West Ave and	South	118	9					9		55	2					-63	4				0 2	One Handicap Space Proposed
curril         35         1.2 </td <td>1</td> <td>12 St</td> <td>Between West Ave and Alton Rd</td> <td>South</td> <td>116</td> <td>9</td> <td></td> <td></td> <td></td> <td></td> <td>9</td> <td></td> <td>115</td> <td>v</td> <td></td> <td></td> <td></td> <td></td> <td>1.</td> <td>4</td> <td></td> <td></td> <td></td> <td></td> <td></td>	1	12 St	Between West Ave and Alton Rd	South	116	9					9		115	v					1.	4					
curl         13         4         7         3         7         3         4 <td>7 5</td> <td>BTOTAL S Between W</td> <td>SOUTH SIDE OF 12 ST Vest Ave and Alton Road</td> <td><u></u></td> <td>234</td> <td>12</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>12</td> <td>0</td> <td>170</td> <td>7</td> <td>0</td> <td>0</td> <td>0</td> <td>0</td> <td>-64</td> <td>ν̈́</td> <td>0</td> <td>0</td> <td>0</td> <td></td> <td>ncludes One Proposed Handicap Space</td>	7 5	BTOTAL S Between W	SOUTH SIDE OF 12 ST Vest Ave and Alton Road	<u></u>	234	12	0	0	0	0	12	0	170	7	0	0	0	0	-64	ν̈́	0	0	0		ncludes One Proposed Handicap Space
corth         156         4         0 </td <td></td> <td>12 St</td> <td>Between West Ave and Alton Rd</td> <td>North</td> <td>55</td> <td>N/A</td> <td></td> <td></td> <td></td> <td></td> <td>3</td> <td></td> <td>7.1</td> <td>т</td> <td></td> <td></td> <td></td> <td></td> <td>16</td> <td>N/A</td> <td></td> <td></td> <td></td> <td></td> <td>One Handicap Space Proposed</td>		12 St	Between West Ave and Alton Rd	North	55	N/A					3		7.1	т					16	N/A					One Handicap Space Proposed
outh         31         4         6         6         7 <td>T</td> <td>12 St</td> <td>Between West Ave and Alton Rd</td> <td>North</td> <td>105</td> <td>4</td> <td></td> <td></td> <td></td> <td></td> <td>0</td> <td></td> <td>0</td> <td>0</td> <td></td> <td></td> <td></td> <td>-</td> <td>-105</td> <td>4</td> <td>-</td> <td></td> <td></td> <td>u 11</td> <td>Parking in front of private parking from Medical Group. Approx. 105' used by vallet</td>	T	12 St	Between West Ave and Alton Rd	North	105	4					0		0	0				-	-105	4	-			u 11	Parking in front of private parking from Medical Group. Approx. 105' used by vallet
outh         31         NA         NA         69         3         9<	1 5	BTOTAL N Between W	NORTH SIDE OF 12 ST West Ave and Alton Road	#	160	4	0	0	0	0	Э	0	7.1	ю	0	0	0	0	68-	N/A	0	0	0		Only 4 spaces are currently marked
outh         11         6         0 <td>1</td> <td>13 St</td> <td>Between West Ave and</td> <td>South</td> <td>91</td> <td>N/A</td> <td></td> <td></td> <td></td> <td></td> <td>S.</td> <td></td> <td>69</td> <td>3</td> <td></td> <td></td> <td></td> <td></td> <td>-22</td> <td>N/A</td> <td></td> <td></td> <td></td> <td></td> <td>One Handicap Space Proposed</td>	1	13 St	Between West Ave and	South	91	N/A					S.		69	3					-22	N/A					One Handicap Space Proposed
outh         123         6         0         124         124         13         13         13         14         13         14	1	13 St	Between West Ave and	South	21	N/A					П		0	0					-21	N/A					Passenger Loading Zone
Orth         1.2         1.2         0<	1	13 St	Between West Ave and Alton Rd	South	121	9					9		108	Ŋ					-13	다					
orth         1         1         2         4		BTOTAL S	SOUTH SIDE OF 13 S' West Ave and Alton Road	벁	233	9	0	0	0	0	12	0	177	∞	0	0	0	0	-56	N/A	0	0	0	0	
orth 3.7 1.	1	13 St	Between West Ave and Alton Rd	North					69	4	4						48	4					-21	0	
orth         17         1         0         0         0         0         0         48         4         -1         -1         0         -39         -1           sest         64         N/A	1	13 St	Between West Ave and Alton Rd	North					18	н	1						0	0					-18	7	
17         1         0         0         0         0         0         0         48         4         -17         -1         0         -39         -1           12         N/A	1	13 St	Between West Ave and Alton Rd	North	17	1					н		0	0					-17	Ţ.					
between 6st and 8st         East         64         N/A	2	BTOTAL   Between	NORTH SIDE OF 13 S West Ave and Alton Road	=1.	17	1	0	0	87	Ŋ	9	0	0	0	0	0	48	4	-17	7	0	0	-39	÷	
between 6st and 8st         East         21         N/A	1	West Ave	between 6st and 8st	East	94	N/A					е		N/A	N/A					N/A	N/A					
between 6st and 8st         East         12         N/A         1         N/A         N/A         N/A	1	West Ave	between 6st and 8st	East	21	N/A					1		N/A	N/A					N/A	N/A					
	1	West Ave	between 6st and 8st	East					12	N/A		Н					N/A	N/A					N/A	N/A	Matorcycle. No sign

Civil W	Civil Works, Inc						WEST A	TAV	NOE	MPR	/ENUE IMPROVEMENTS	ENTS		- SOUTH PROJECT	ROJ	ECT								May 10, 2020
	-						EXIS	EXISTING						PROPOSED	SED		1						l	
The second secon						Type of Parking	Parking							Type of Parking	rking				Net Ga	Net Gain/Loss of Parking [LF]	arking [LF]		П	
				Parallel	lel	Perpendicular	dicular	Angle	<u>e</u>	Spaces	Spaces Occupied	Parallel	lel	Perpendicular	ular	Angle		Parallel		Perpendicular	ılar	Angle		
Number	Name	Limits	Side	E	# of Marked Spaces	E	# of Marked Spaces	F	# of Marked Spaces	Cars	Motorcycle	Ħ	# of Marked Spaces	FT N	# of Marked Spaces	E	# of Marked Spaces	FT St M	# of Marked Spaces	FT Ng	# of Marked Spaces	FT Ma	# of Marked Spaces	Comments
4	West Ave	between 6st and 8st	East	109	N/A					Z.		N/A	N/A					N/A	N/A					
w	West Ave	between 6st and 8st	East					13	N/A	0						N/A	N/A					N/A	N/A Mo	Motorcycle
9	West Ave	between 6st and 8st	East	67	N/A					ю		N/A	N/A					N/A	N/A					
IS*	UBTOTAL	*SUBTOTAL EAST SIDE OF WEST AVE=	/E=	261	0	0	0	25	0	12	1	N/A	N/A	0	0	N/A	N/A	N/A	N/A	0	0	N/A	N/A The	There is no proposed work on this area
12	West Ave	Between 8th Stand 9th St	East	48	N/A					æ		0	0					-48	N/A					
13	West Ave	Between 8th Stand 9th St	East	95	N/A					8		0	0					-56	N/A					
14	West Ave	Between 8th St and 9th St	East	40	2						111	0	0					-40	-2					
ร	JBTOTAL E	SUBTOTAL EAST SIDE OF WEST AVE=	<u>ٿ</u>	144	2	0	0	0	0	ø	11	0	0	0	0	0	0	-144	N/A	0	0	0	o Cour	Only two spaces are currently marked
22	West Ave	Between 9th St and 10th St	East	30	N/A					н		0	0					-30	N/A				Ŝ	Commercial Loading Zone
23	West Ave	Between 9th St and 10th St	East	223	N/A					11		0	0					-223	N/A					
24	West Ave	Between 9th St and 10th St	East			22	4				11			0	0					-22	4		Moto	Motorcycle. No Motorcycle Sign
25	West Ave	Between 9th Stand 10th St	East	26	1					1		0	0					-26	-1				No No	Shaded Hadicap Markings. No Handicap Sign
76	West Ave	West Ave Between 9th St and 10th St	East	31	н					п		0	0					-31	ţ.					
S	UBTOTAL	SUBTOTAL EAST SIDE OF WEST AVE=	/E=	310	2	22	4	0	0	14	11	0	0	0	0	0	0	-310	N/A	-22	4	0	O We	Only two car spaces and 4 Motorcycle spaces are currently marked
37	West Ave	Between 10th St and 11 St	East	148	7					7		0	0					-148	-2			1	1	
38	West Ave	Between 10th St and 11 St	East	42	N/A						16	0	0					-42	N/A					
S	UBTOTAL Be	SUBTOTAL EAST SIDE OF WEST AVE=	/E=	190	7	0	0	0	0	7	16	0	0	0	0	0	0	-190	N/A	0	0	0	O E	Only 7 spaces are currently marked
47	West Ave	Between 11 St and 12 St	East	100	4					1		0	0					-100	4	-		7	Ŧ.	Freight Loading Zone
48	West Ave	e Between 11 St and 12 St	East	109	N/A					m	18	0	0					-109	N/A					

Civil W	Civil Works, Inc						WEST		ENDE	IMPF	<b>AVENUE IMPROVEMENTS - SOUTH PROJECT</b>	ENTS	10S-	H	PROJ	ECT								May 10, 2020
							EXIS	EXISTING						PROPOSED	SED									
						Type of	Type of Parking							Type of Parking	arking				Net G	iain/Loss of	Vet Gain/Loss of Parking [LF]	-1		
Pocation	Street			Parallel	lei	Perpendicular	dicular	Anı	Angle	Spaces	spaces Occupied	Parallel	llef	Perpendicular	cular	Angle	d)	Parallel	el	Perpendicular	icular	Angle	e l	- decommon
Number	Name	Limits	Side	E	# of Marked Spaces	ь	# of Marked Spaces	Ħ	# of Marked Spaces	Cars	Motorcycle	ь	# of Marked Spaces	E	# of Marked Spaces	E	# of Marked Spaces	E	# of Marked Spaces	E E	# of Marked Spaces	Ŀ	# of Marked Spaces	
SU	BTOTAL E	SUBTOTAL EAST SIDE OF WEST AVE=	Ш	209	4	0	0	0	0	4	18	0	0	0	0	0	0	-209	N/A	0	0	0	0	Only 4 spaces are currently marked
53	West Ave	Between 12 St and 13 St	East	99	N/A					4		0	0					99-	N/A					
54	West Ave	Between 12 St and 13 St	East	46	N/A					1	12	0	0					-46	N/A					
55	West Ave	Between 12 St and 13 St	East	20	1					П		0	0					-20	7				_	Passenger Loading Zone
ns	BTOTAL E	SUBTOTAL EAST SIDE OF WEST AVE=	ü	132	н	0	0	0	0	9	12	0	0	0	0	0	0	-132	N/A	0	0	0	0	Only 1 space is currently marked
62	West Ave	Between 13 St and 14 St	East	65	N/A					9		0	0					-65	N/A					
63	West Ave	Between 13 St and 14 St	East	47	N/A					2		0	0					-47	N/A					
64	West Ave	Between 13 St and 14 St	East	31	N/A					2		0	0					-31	N/A					
ns	BTOTAL E	SUBTOTAL EAST SIDE OF WEST AVE=	#	143	N/A	0	0	0	0	7	0	0	0	0	0	0	0	-143	N/A	0	0	0	0	
59	West Ave	Between 13 St and 14 St	West	99	N/A					0		0	0					-65	N/A					
ns	BTOTAL V	SUBTOTAL WEST SIDE OF WEST AVE= Between 13 St and 14 St	/E=	99	N/A	0	0	0	0	0	0	0	0	0	0	0	0	-65	N/A	0	0	0	0	

## APPENDIX 2 Parking Restriction Standards

Select Year: 2021 ✓ Go

## The 2021 Florida Statutes

Title XXIII

#### Chapter 316

View Entire Chapter

MOTOR VEHICLES

STATE UNIFORM TRAFFIC CONTROL

#### Stopping, standing, or parking prohibited in specified places.-316.1945

- (1) Except when necessary to avoid conflict with other traffic, or in compliance with law or the directions of a police officer or official traffic control device, no person shall:
  - (a) Stop, stand, or park a vehicle:
  - 1. On the roadway side of any vehicle stopped or parked at the edge or curb of a street.
  - 2. On a sidewalk.
  - 3. Within an intersection.
  - 4. On a crosswalk.
- 5. Between a safety zone and the adjacent curb or within 30 feet of points on the curb immediately opposite the ends of a safety zone, unless the Department of Transportation indicates a different length by signs or markings.
- 6. Alongside or opposite any street excavation or obstruction when stopping, standing, or parking would obstruct traffic.
  - 7. Upon any bridge or other elevated structure upon a highway or within a highway tunnel.
  - 8. On any railroad tracks.
  - 9. On a bicycle path.
  - 10. At any place where official traffic control devices prohibit stopping.
- 11. On the roadway or shoulder of a limited access facility, except as provided by regulation of the Department of Transportation, or on the paved portion of a connecting ramp; except that a vehicle which is disabled or in a condition improper to be driven as a result of mechanical failure or crash may be parked on such shoulder for a period not to exceed 6 hours. This provision is not applicable to a person stopping a vehicle to render aid to an injured person or assistance to a disabled vehicle in obedience to the directions of a law enforcement officer or to a person stopping a vehicle in compliance with applicable traffic laws.
- 12. For the purpose of loading or unloading a passenger on the paved roadway or shoulder of a limited access facility or on the paved portion of any connecting ramp. This provision is not applicable to a person stopping a vehicle to render aid to an injured person or assistance to a disabled vehicle.
- (b) Stand or park a vehicle, whether occupied or not, except momentarily to pick up or discharge a passenger or passengers:
  - 1. In front of a public or private driveway.
  - 2. Within 15 feet of a fire hydrant.
  - 3. Within 20 feet of a crosswalk at an intersection.
- 4. Within 30 feet upon the approach to any flashing signal, stop sign, or traffic control signal located at the side of a roadway.
- 5. Within 20 feet of the driveway entrance to any fire station and on the side of a street opposite the entrance to any fire station within 75 feet of such entrance (when property signposted).
  - 6. On an exclusive bicycle lane.
  - 7. At any place where official traffic control devices prohibit standing.

- (c) Park a vehicle, whether occupied or not, except temporarily for the purpose of, and while actually engaged in, loading or unloading merchandise or passengers:
- 1. Within 50 feet of the nearest rail of a railroad crossing unless the Department of Transportation establishes a different distance due to unusual circumstances.
  - 2. At any place where official signs prohibit parking.
- (2) No person shall move a vehicle not lawfully under his or her control into any such prohibited area or away from a curb such a distance as is unlawful.
- (3) A law enforcement officer or parking enforcement specialist who discovers a vehicle parked in violation of this section or a municipal or county ordinance may:
  - (a) Issue a ticket form as may be used by a political subdivision or municipality to the driver; or
- (b) If the vehicle is unattended, attach such ticket to the vehicle in a conspicuous place, except that the uniform traffic citation prepared by the department pursuant to s. <u>316.650</u> may not be issued by being attached to an unattended vehicle.

The uniform traffic citation prepared by the department pursuant to s. <u>316.650</u> may not be issued for violation of a municipal or county parking ordinance.

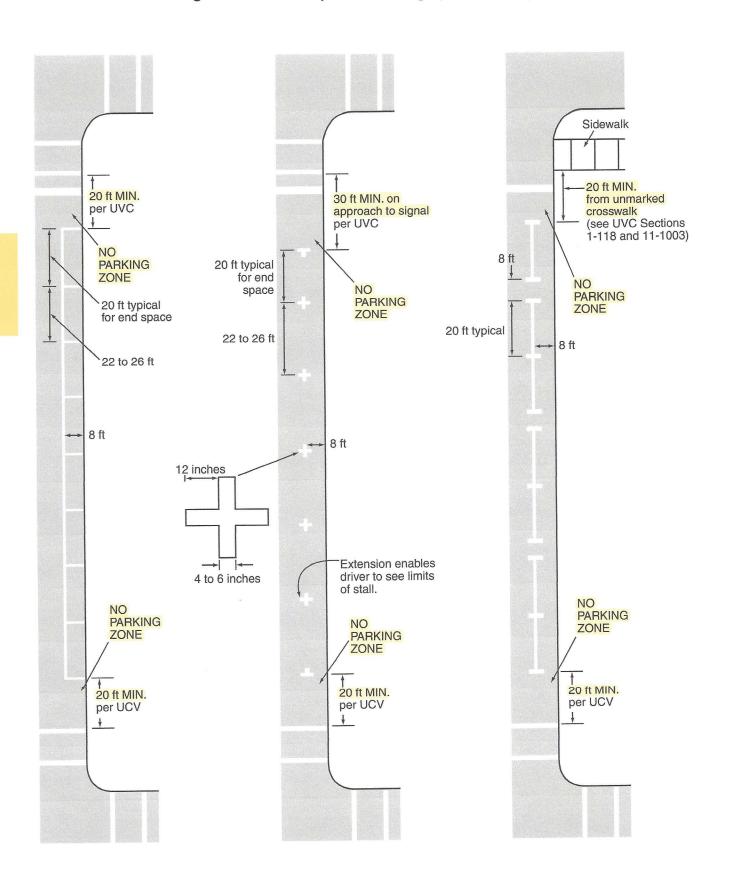
(4) A violation of this section is a noncriminal traffic infraction, punishable as a nonmoving violation as provided in chapter 318.

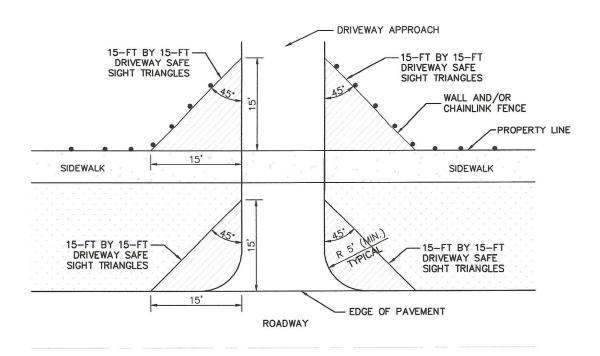
History.—s. 1, ch. 71-135; s. 1, ch. 76-31; s. 2, ch. 78-52; s. 1, ch. 79-403; s. 2, ch. 80-316; s. 5, ch. 83-68; s. 5, ch. 84-309; s. 1, ch. 85-81; s. 2, ch. 88-91; s. 317, ch. 95-148; s. 143, ch. 99-248.

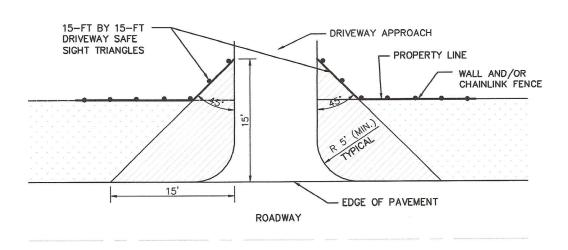
Note.-Former s. 316.160.

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Figure 3B-21. Examples of Parking Space Markings







THE HEIGHT OF FENCES, WALLS, BUS SHELTERS AND HEDGES SHALL NOT EXCEED 2 FEET IN HEIGHT WITHIN FIFTEEN (15) FEET OF THE EDGE OF DRIVEWAY LEADING TO A PUBLIC RIGHT—OF—WAY.

THE SAFE SIGHT DISTANCE TRIANGLE AREA SHALL NOT CONTAIN OBSTRUCTIONS TO CROSS—VISIBILITY AT A HEIGHT BETWEEN 2 FEET AND 6 FEET ABOVE PAVEMENT, POTENTIAL OBSTRUCTIONS INCLUDE, BUT ARE NOT LIMITED TO STRUCTURES, GRASS, GROUND COVERS, SHRUBS, VINES, HEDGES, TREES, ROCKS, WALLS AND FENCES.

DRIVEWAY SAFE SIGHT TRIANGLE N.T.S.

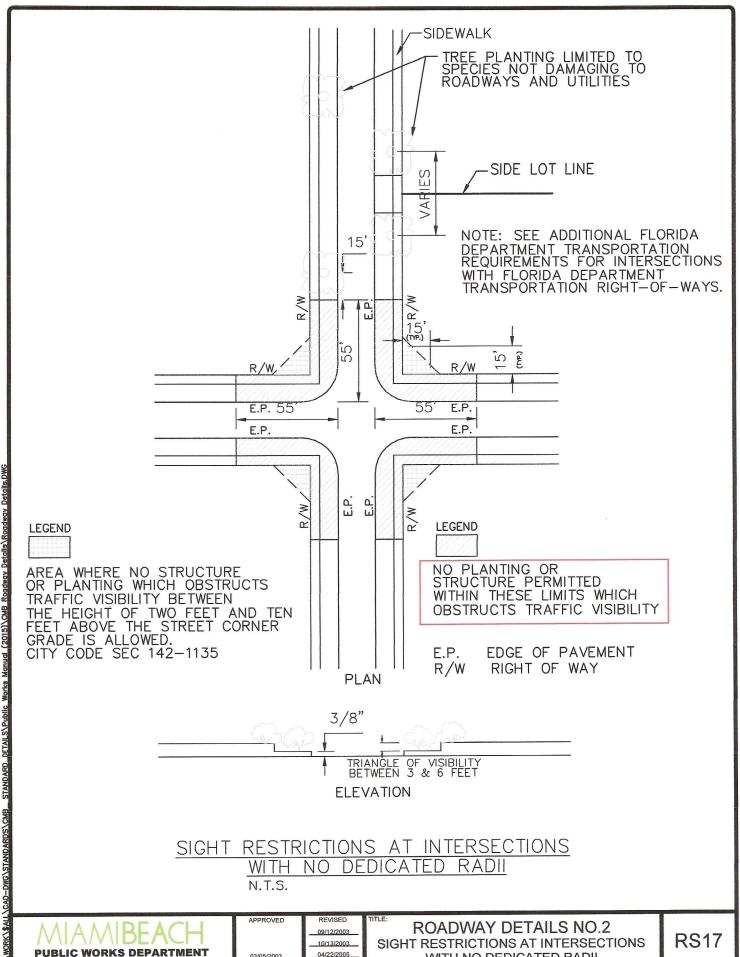
MIAMIBEACH
PUBLIC WORKS DEPARTMENT

1700 CONVENTION CENTER DRIVE, MIAMI BEACH, FL.33139

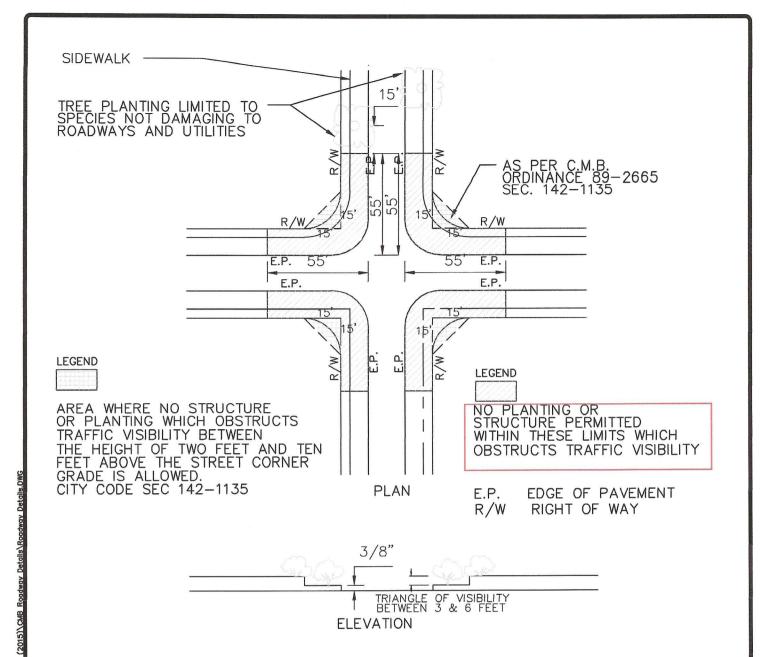
APPROVED

REVISED 04/08/2011 05/29/2013 07/14/2015 TITLE:

ROADWAY DETAILS NO.2 DRIVEWAY SAFE SIGHT TRIANGLE



WITH NO DEDICATED RADII



NOTE:

FOR 30 M.P.H. RESIDENTIAL STREETS. SEE ADDITIONAL FLORIDA DEPARTMENT OF TRANSPORTATION REQUIREMENTS FOR INTERSECTIONS WITH FLORIDA DEPARTMENT OF TRANSPORTATION RIGHT—OF—WAYS.

## SIGHT RESTRICTIONS AT INTERSECTIONS WITH DEDICATED RADII N.T.S.

PUBLIC WORKS DEPARTMENT
1700 CONVENTION OF PUTER PRIVE MIAMI BEACH, FL 33139

APPROVED

09/12/2003 10/13/2003 04/22/2005 ROADWAY DETAILS NO.2
SIGHT RESTRICTIONS AT INTERSECTIONS
WITH DEDICATED RADII