

Plan Address: 411 Michigan Ave, Miami Beach, FL 33139-6509
Applicant: SOFI Owner, LLC
Applicant Address: 55 E. 59th Street, Floor 24
 New York, NY 10022

Architect: CUBE 3, LLC
Architect Address: 111 SW 3rd St., Floor 4, Miami, FL 33130

Date: 29 November 2021

Checklist Comments Report – Response

Reviewer: Irina Villegas

Comments: City Comments issued 10/29/2021
 Re-submitted with responses on 11/15/2021
 Re-submitted with Debbie Tackett's notes 12/3/2021

Project Name: 411 Michigan Ave

HPB Plan Version 2 Corrections Report Response (HPB21-0486)

1 *Status: Met - per comments received on 10/29/21*
C3 Response: 11/01/2021: Confirmed

2 *Status: Met - per comments received on 10/29/21*
Planning Dept. Comments issued on 10/29/21
Survey shall indicate grade elevation fronting 5th Street.

C3 Response: 11/01/2021
 The survey indicates a grade elevation of +3.19' fronting 5th street. It is at the northwest corner of the property line at the sidewalk.

3 *Status: Met - per comments received on 10/29/21*
Planning Dept. Comments issued on 10/29/21
The project requires 5'-0" setback from the rear. Variance not requested.

C3 Response: 11/01/2021
 Page A2.3 indicates a Rear Setback on the Alley side of the property of 9'-0". We are not considering the South side of the property line as the Rear Setback. We are considering the East side of the property the Rear as it is fronted by Michigan Ave on the West. The previously approved permit set from 2015 for the hotel (existing foundation) followed our same positioning logic and this has been confirmed by Deborah Tackett in multiple meetings.

#4 *Status: Met - per comments received on 10/29/21*
Planning Dept. Comments issued on 10/29/21
Page A2.1. Revise grade elevation to match elevation noted in survey, as noted above.

C3 Response: 11/01/2021
 Grade is approximately 4' NGVD and will be verified and properly documented for

building permit. This does not impact the FFE and height of building.

#5

Status: Met - per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.1. Revise proposed rear setback. Proposed rear setback is 1'-6".

C3 Response: 11/01/2021

We are not considering the South side of the property line as the Rear Setback. We are considering the East side of the property the "rear" as it is "fronted" by Michigan Ave on the West. This is because the address is 411 Michigan Ave and the front approach is off Michigan Ave. The previously approved permit set from 2015 for the hotel (existing foundation) followed our same positioning logic and this has been confirmed by Deborah Tackett in multiple meetings.

#6

Status: Met - per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.2. Revise required parking for office. 38,461 sf requires 1 space per 400 sf of area or 97 parking spaces. Total required parking spaces is 106 ps. Indicate area of retail space on each retail space to verify calculation. Retail area on historic structure can be removed from the overall retail calculation for parking.

C3 Response: 11/01/2021

Please see revised page A2.2. Total parking count required adjusted to 106 spaces. Please also see ground floor plan denoting SF of each retail space. Retail parking count has been removed from the calculations.

#7

Status: Met - per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.3 indicate provided setbacks.

C3 Response: 11/01/2021

Page A2.3 has been removed per PB Comments to remove zoning sheets outside of CMB Zoning. Only City of Miami Beach showing is included in the final HPB package. Provided setbacks can be seen on A2.1.

#8

Status: Acknowledged, see response to comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.4. There is no office space at the ground floor. Recalculate parking. Long term bicycle parking (30) is not consistent with indicated on page A2.2 (10)

C3 Response: 11/01/2021

The lobby is being counted as ground floor office space. The long-term bicycle parking can be seen in the cellar on page A3.2.

#9

Status: Met - per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.5. Indicate front, street side, rear and interior side (facing alley)

C3 Response: 11/01/2021

Please see revised page A2.5. We are not considering the South side of the property

line as the Rear Setback. We are considering the East side of the property the “Rear” as it is “Fronted” by Michigan Ave on the West. This is because the address is 411 Michigan Ave and the front approach is off Michigan Ave. The previously approved permit set from 2015 for the hotel (existing foundation) followed our same positioning logic and this has been confirmed by Deborah Tackett in multiple meetings.

#10 Status: Met - per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.7. and page A2.8. Alley cannot be used as a two-way as the alley is not 22'-0" in width and it is only one-way drive. Show structural columns and walls on basement plans. Elevator walls shall be setback 18" from the drive aisle when adjacent to a parking space.

C3 Response: 11/01/2021

Acknowledged. Please see revised plans C1.1-C1.6. We are not proposing the 20'-0" alley be a 2-way street. Traffic studies are revised to keep public streets operating the way they are presently planned as the alley going southbound from 5th Street.

#11 Status: Met – met as per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Page A2.13 stairs , elevator, bathroom count in FAR. Bicycle parking shall be in a secured enclosure or the area shall count in FAR.

C3 Response: 11/01/2021

The bicycle parking is below grade and the ceiling of the enclosure is below grade, the ceiling of the bathroom is below grade. The bathroom ceiling is at approximately N.G.V.D +4', and for permitting will be below grade so these areas do not count as FAR. The elevator will be counted. All treads and risers of stairs that are above grade are accounted for on the ground floor plan for FAR. See section on page A4.12.

#12 Status: Acknowledged - per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Project shall comply with section 142-699 (c). An open courtyard, open to the sky shall be provided. Variance not requested.

C3 Response: 11/01/2021

The Applicant requests a variance of the open court requirement. The simple yet elegant design of the office building would be negatively impacted by an open court. However, the design captures the intent of the Code by providing a deep plaza at ground level, with a clear opening of approximately 36 feet from Michigan Avenue, including a 22'-6" tall, 2,200 square foot breezeway and 9'-0" deep balconies above, also with high ceiling heights for light and air to permeate the frontage. Further, 566 square feet of open space, not including the stairs, is located in front of the relocated historic building. Additionally, the building has been designed to address sea level rise and be resilient. Placing portions of the building and the abutting areas below design elevation risks flooding to the Property and the surrounding right of way.

#13 Status: Acknowledged per comments received on 10/29/21

Planning Dept. Comments issued on 10/29/21

Project does not comply with section 133-61. a)(1) d. and f. fronting Michigan Avenue. Light pole on Michigan shall be relocated.

C3 Response: 11/01/2021

The circulation zone matches conditions along Michigan Ave to the South, maintaining a 8'-5" width or wider. This circulation zone is free from obstructions clear pedestrian path to be delineated as suggested. Please see page A3.1 and Landscape pages beginning L1.1.

#14 **Status:** Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21

Page A3.1. Provide overall dimensions of the retail spaces. Label all rooms/spaces. Driveway width is not consistent with page A2.18. Please coordinate. Transformer is recommended in an enclosed room.

C3 Response: 11/01/2021

Please see revised A3.1. The pad mounted transformer now has an architectural screening around it. Additionally, see revised ground floor plan on page A3.3.

#15 **Status:** Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21

Page A3.2. Dimension all parking spaces. A note indicating dimension of all parking is acceptable. Dimension driveway. Indicate location of structural columns and walls. Columns and walls shall be setback 18" from the two-way driveway for perpendicular parking. Clearly note property lines.

C3 Response: 11/01/2021

Please see revised A3.2 that provides all requested dimensions.

#16 **Status:** Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21

Page A3.3. parking spaces with columns in between cannot reduce the parking spaces to less than 8'-0". Clearly indicate width of parking spaces column to column. Clearly define dimensions of loading space (s) adjacent to the alley. Indicate area in square feet of each retail space. Call out section drawings are missing.

C3 Response: 11/01/2021, adjusted 12/03/21

Please see revised A3.2, A3.3. No parking spaces are less than 8'-6". Loading pullover space from alley, to allow vehicles to pass is dimensioned as 10'-0" wide (5'-0" on property and 5'-0" in alley) by 30'-0" long accepted as per conversation with Parking Director on 12/3/21. All retail spaces labeled with appropriate SF. All section and elevation call out have been provided.

#17 **Status:** Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21

Loading plan approved by the parking Department shall be provided to waive the required loading requirements. Indicate expected areas used for loading on the street.

C3 Response: 11/01/2021, adjusted 12/03/21

There is a loading space adjacent to the alley on page A3.3, accepted as per conversation with Parking Director on 12/3/21. Fire Dept. to confirm access from alley.

#18 Status: Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21
Page A3.4. Specify type of amenity proposed. Open retail area at the back is not allowed. Call out section pages are missing.

C3 Response: 11/01/2021

See revised A3.4. The amenity is a private fitness space for office tenants. We have revised the proposal to remove the open retail area at the back. We have revised the proposal to call out sections.

#19 Status: Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21
Page A3.8. Clearly show dimensions of generator proposed.

C3 Response: 11/01/2021

See revised A3.8. Generator removed from rooftop and will be placed within space labeled as emergency electric within an enclosed room on the podium level.

#20 Status: Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21
Page C1.1. length of parking spaces shall match the length of the car indicated.

C3 Response: 11/01/2021

See revised C1.1.

#21 Status: Acknowledged as per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21
Building height for the property is 50'-0" as it is east of Lenox Avenue.

C3 Response: 11/01/2021

To achieve the Class A Office component, the Applicant is working with the City on a Code Amendment to allow office uses at 75 feet where currently limited to 50 feet for this localized area by Jefferson and Michigan Avenues. Notably residential and hotel uses, which are not proposed at the project, can be built at 75 feet on the Property without a Code Amendment. The purpose is not for extra floors, rather for the additional floor to ceiling heights necessary to attract Class A office tenants.

#22 Status: Met - per comments received on 10/29/21
Planning Dept. Comments issued on 10/29/21
Alley cannot be used as a two-way driveway. It does not have 22'-0" in width and it also have light poles, and power poles as noted on plans.

C3 Response: 11/01/2021

Acknowledged. We have revised the proposal to allow all public right of ways to operate at their existing capacity. The alley is not a two-way driveway in this proposal, and neither is the private driveway. Please see pages C1.1- C1.6.

#23 Status: Met - per comments received on 10/29/21
 Planning Dept. Comments issued on 10/29/21

C3 Response: 11/01/2021

Please see revised survey dated 10/05/2021 on page A1.1

6. HPB Plan Review (11-15-21)

Reviewer: James Seiberling

Comments: DEFICIENCIES IN PRESENTATION

- a. *Provide additional information relative to the existing structural condition of the contributing building proposed to be retained and relocated. This shall also include a methodology for shoring and moving the building.*

C3 Response: 11/01/2021:

Once the permit approval phase is occurring, a Construction Manager would determine the appropriate method sub-contract a specialist to diagnose the structural condition of the existing building and prescribe a method to shore and lift the building. We have contacted a contractor who provides this service and additionally Our structural engineer has outlined the general process as follows:

1. Provide a foundation for the historic building in its final location, the top of the slab about 5' below the finished floor.
2. The contractor will shore/brace the interior of the house prior to relocating.
3. Once lifted, steel I beams are wheeled under and the house wheels to its final location. Shoring is placed under, and the stem walls/pedestal system is built on the mat foundation to receive the house structure.

- b. *Provide line drawn elevations in addition to the rendered elevations.*

C3 Response: 11/01/2021:

Please see line drawn elevations after the rendered elevations at page A4.7

- c. *Provide in narrative and/or visual form, the design inspiration for the designs of the buildings including how they relate to the fabric and architectural heritage the surrounding historic district and greater Miami Beach.*

C3 Response: 11/01/2021:

Please see design architect's narrative on page A6.2. Additionally, the response below:

411 Michigan is a "gateway" building on 5th Street, one of the most prominent vehicular arteries in Miami Beach. 411 Michigan is also placed in a significant historic neighborhood where several key architectural styles from different times converge. In recognizing this unique opportunity, we responded to the site's recognizable position and historical ties by commissioning a master contemporary Architect who's designs profoundly recognize both the past and the future. We established the following principles that each respond to a particular architectural tradition in South Miami Beach: symmetry (Art Deco), activated outdoor space (Mediterranean Revival), and minimalism (MiMo). These principles become the foundation and genesis of our design for 411

Michigan.

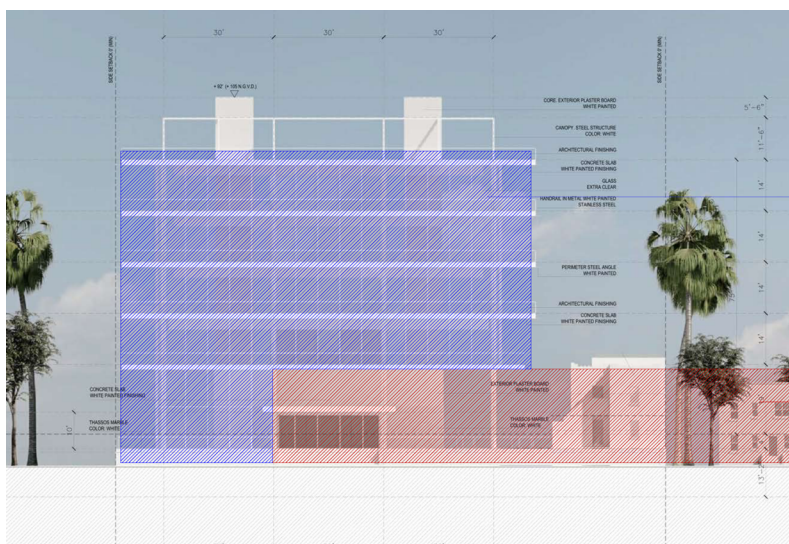
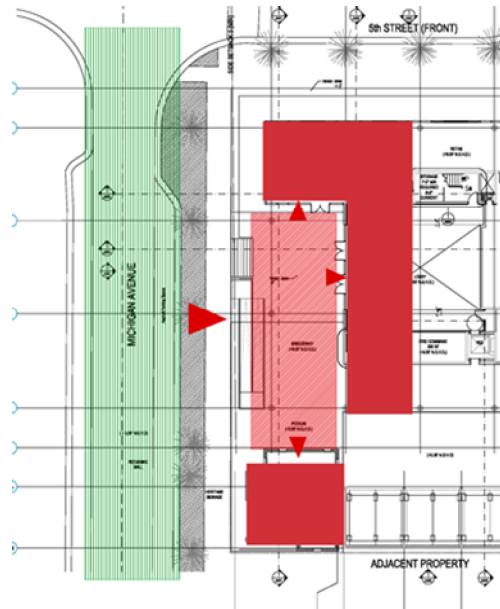
Symmetry is an essential characteristic to the Art Deco style. The plan, elevation and sectional diagram of the building are all cleanly and identifiably symmetrical.

Mediterranean Revival features “courtyard[s] and arcades, loggias and porches to provide easy access to the outdoors”. Mediterranean Revival is reflected in this building through the open to the sky courtyard at the 3rd Level, the Entry Plaza, and the large wrapping balconies on every floor.

MiMo’s Design Guidelines reference courtyards and open-air corridors that directly speaks to the contextual plan diagram and demands from climate and use; all of which are elements utilized in our proposal for the new office building at 411 Michigan. MiMo’s Design Guidelines also reference breeze blocks to reference Art Deco, which are the key building element of our stand alone garage behind the historic house. The design of our building coincides within the design guidelines for this neighborhood and the context of South Beach.

The plaza at the entry podium is the first area where visitors would be able to experience all programs of our site (office, retail, and historic house retail) and functions as a congregational/buffer zone from eastbound traffic approaching on 5th Street. Therefore, the north-facing historic house is more consistent with the site plan logic and aligns with the city’s goal to promote activated semi-public spaces.

The new building is designed with modest ‘transparent’ materials; the window assembly is built with butt-mullions (structure behind the glass) to appear floating. The wrap around balconies aid in making the glass-enclosed space to appear as something between indoor and outdoor; giving a lightness and airiness to the structure. It does not ‘overwhelm’ the



"transparent" surfaces that presents the 'future' to the 5th

"solid" surfaces with contextual scale and "historic" language

historic house, but rather creates a natural transition from the 'solid'/low-rise historic context, to the 'permeable'/mid-rise facing the major gateway street. The two scales are linked with a formal language of modern minimalism and a gestural consistency with the 2nd floor balcony and the roof of the historic building being co-planar.

- d. *The detail drawings at the end of the package are not legible on an 11x17 sheet.*

C3 Response: 11/01/2021:

Acknowledged. Please see revised detail drawings.

- e. *Clarify the use of the outside retail area at the mezzanine level.*

C3 Response: 11/01/2021:

We have revised the proposal to be an outdoor terrace at the mezzanine. It is no longer proposed as retail space.

- f. *Explain the intent of the steel angle at the balcony slab edges.*

C3 Response: 11/01/2021:

The design intent of the steel angle at the balcony slab edges is to visually dissolve the profile of the slab edge to thin air. This detail reinforces the concept this building is floating, light, and transparent.

2. DESIGN APPROPRIATENESS COMMENTS (Recommendations)

- a. *Staff recommends that the landscaping plan be further developed and substantially enhanced including increasing the amount of ground level landscape material.*

C3 Response: 11/01/2021:

Confirmed. We have substantially increased the amount of greenery on site, please see page A3.1 and Landscape package beginning on page L1.1

- b. *Staff recommends that the relocated contributing building be elevated to at least BFE.*

C3 Response: 11/01/2021:

Confirmed. We have revised the proposal to have the historic building be raised to +9 NGVD. See sheet A3.3

- c. *Staff recommends that the application explore expanding the below grade parking to the lot to the south in order to eliminate the additional garage structure and increase the amount of green area on the site and to provide additional area to transition to an elevated contributing building.*

C3 Response: 11/01/2021:

We have increased the amount of green area on the site and elevated the contributing historic building as per staff's request. However, to reduce the environmental impact to the site, the team has proposed to reuse the existing foundation without disturbance as to minimally disturb the remainder of the site. The adaptive reuse of the existing structure also allows the project to be economically feasible for ownership as well as working with existing approvals to accommodate parking. In addition, sustainability measures are being proposed, such as pervious pavers along the private drive, a green roof, and green walls on

the new parking structure, and increased landscaped area in front of the relocated historic home.

- d. *Staff recommends retaining the orientation of the relocated contributing structure and the elimination of the driveway.*

C3 Response: 11/01/2021:

The home will retain its original orientation and be raised to NGVD 9'-0", as per staff's request. See Section A5 for Historic Building Documentation, and elevations on pages A4.7-A4.10.

- e. *Staff recommends that the design for the mechanical parking structure be further developed.*

C3 Response: 11/01/2021:

We have placed an opaque screen behind the openings in the façade in regard to the breeze block and provided details that ensure the parking mechanisms will be screened from view. The use of breezeblock ties into the existing contextual architecture of the area and provides the site an architecturally transitional building that links the historical building with the contemporary office building. See sheet A7.2.

- f. *Staff recommends that the design of the new office building be further developed including the exploration of reduced balcony projections. At a minimum, staff believed that the balconies along the south side area having an adverse impact on the relocated contributing building.*

C3 Response: 11/01/2021:

We are reducing the balcony projection on the south side of the property by half it's original dimension of 10' to be 4'6" to provide ample space to the historic building, and to frame the building on the site.

- g. *Staff recommends the exploration of a more durable, equally high-quality material for the base of the building.*

C3 Response: 11/01/2021:

Team will consider porcelain or similar synthetic material as substitute to be more durable and less porous. See materials sheet for proposed alternates. See sheet A7.2. for note stating Thassos Marble or synthetic equivalent.

- h. *Staff recommends that the historical evaluation be placed on the exterior of the contributing building and not freestanding.*

C3 Response: 11/01/2021:

Confirmed, the Historical signage to be moved on to front façade of contributing building as a placard. See sheet A3.1 and renderings on sheet A5.12 and A5.13.

8. Public Works – LUB Review (11-19-21)

Reviewer: Aaron Osborne

1. *Since this is a Non-Residential property, an analysis of the water and sewer system may be required prior to connecting to the City's Utility Infrastructure to confirm if the system can accommodate the additional flows proposed by the increased demand from the site. Submit the following information to AaronOsborne@miamibeachfl.gov.*
 - A. *Demand flows in a signed and sealed letter (by a Florida PE) for each property (if multiple sites).*
 - i. *Commercial Water Demands*
 - ii. *Residential Water Demands*
 - iii. *Fire Flow Demand*
 - iv. *Irrigation Demand*
 - B. *If demand flows exceed 10,000 GPD a Hydraulic model will not be required.*

C3 Response: 12/02/2021: Acknowledged, these deliverables will be provided as part of permit drawings.

2. *Corner properties must comply with Sec. 142-1135. - Corner visibility. On a corner lot, there shall be no structure or planting which obstructs traffic visibility between the height of two feet and ten feet above the street corner grade, within the triangular space bounded by the two intersecting right-of-way lines and a straight line connecting the right-of-way lines 15 feet from their intersection. Refer to CMB Public Works Standard detail 10-26. (Sheet A3.1)*
 - A. *Declaration of Restrictive Covenant will be required if the project is not able to comply with this requirement.*

C3 Response: 12/02/2021: Please see revised site plan on page A3.1 where site triangles are shown.

3. *Driveways with entrance/exits or exits only will need to have 15 feet sight-visibility per CMB standard detail 10-23. Note: This does not apply to entrance only driveways. (Sheets A2.1, A3.1, A4.7, L1.3 & L1.4)*
 - A. *If the sight-visibility cannot be received, a Hold Harmless Agreement and Convex mirrors will be required prior to approval of the building permit. Convex mirrors must be approved by the Planning and Zoning Department.*
 - B. *Remember no obstructions from 2 feet to 6 feet above grade are allowed within the sight triangles.*

C3 Response: 12/02/2021: Acknowledged. Convex mirrors are provided on the southwest corner of the Podium, North of the private drive. Please see A3.1 and A3.3.

4. *Driveway aprons are to be a minimum of 12' per Public Works Manual standard details 10-24. (Sheets A2.4)*

C3 Response: 12/02/2021: Provided. Please see plan on A3.1.

Permitting Notes:

1. *All stormwater runoff must be retained within your private property and the any proposed on-site stormwater system must hold a 10-year, 24-hour rainfall event with an intensity of 8.75 inches of rainfall.*
 - A. *Driveway entrance/exit may need trench drains to achieve this requirement.*

C3 Response: 12/02/2021: We will coordinate with the Civil Engineer and provide if necessary.

2. *In the future the city will be raising the elevation of the sidewalk in this area to 3.7 NAVD (5.26 NGVD). Consider this information in your decisions based on your design.*

C3 Response: 12/02/2021: The revised site plan takes this into account and shows raised and sloped sidewalks on Michigan Ave as well as the Private Drive; which is raised to 5.26' NGVD to prepare for the future street elevations of Michigan and the Alleyway to the east.

3. *Ensure the foundations of the steps on Michigan Ave is constructed entirely inside your private property. (Sheet A4.10)*

C3 Response: 12/02/2021: Confirmed, the steps are entirely within the property.

4. *Any improvements proposed along 5th Street will require approval from FDOT prior to receiving a Public Works Department Right of Way Permit (Sheet L1.4)*

C3 Response: 12/02/2021: We will coordinate accordingly.

5. *Maintenance Agreement is required for proposed Landscape in the public right of way prior to TCO (Sheets L1.3 and L1.4)*

C3 Response: 12/02/2021: We will coordinate accordingly.

9. Urban Forestry Group Review (11-19-21)

Reviewer: Omar Leon

A Tree evaluation report conducted by a Certified Arborist or registered Consulting Arborist shall be provided for review prior to approval. In addition to a standard tree evaluation, the report shall provide detailed narrative as to how trees will be impacted and what measures will be taken to preserve these trees during construction and per city code. Tree pruning and Management plan shall be provided for review to assure proper tree care is being taken based on proposed work.

Proposed work would require a substantial amount of infrastructure such as Soil cells, and suspended pavement systems for review and approval by Urban Forestry. This shall be a requirement of the development orders. Soil volume requirements is a requirement of city code. 5th Street shall consist of Green Buttonwood Trees with a minimum size of 18 ft ht and 4 inch Caliper. Suspended Pavements and or Soil Cells must be used.

Revised plans and reports must be submitted for review. I am interested in looking at the Live Oaks on the south west of the property.

Finally tree number 5 must have species updated to reflect existing conditions.

C3 Response: 12/02/2021:

Files of the arborist report included in the submission are attached. Also refer to sheets L1.2 and L1.3. Regarding the trees being impacted, tree pruning and management plan; all trees and palms on the site and on the adjacent right of way are proposed to be removed. A combination of open planting areas (swale areas), soil cells, and/or CU Structural Soil will be specified as needed to achieve proper soil volume for tree roots once the final street tree species are selected. The tree species and size will be addressed during the Building and FDOT Permitting Phase. We will coordinate a site meeting with the Urban Forester. Tree #5 was pruned by the City. The Arborist Report

will be updated with notes and photos to reflect the current condition of Tree #5 post-pruning by the City.

10. Fire Review (10-19-21):

Reviewer: Enrique Nunez

Initial Review

1. *Not enough information for a Fire Review.*

C3 Response: 12/02/2021: Noted

2. *The plan must include Design information on the proposed Life Safety Systems, for review.*

C3 Response: 12/02/2021: This plan will be provided as part of permit drawings.

3. *Identify the location of the Fire Main and Fire Hydrants in the area to determine if proper water supply is available.*

C3 Response: 12/02/2021: There is a fire hydrant across the street from the NW corner of the site on the west side of Michigan Ave. This hydrant is not shown on the survey. A Siamese connection is to be provided to service our site; please see West Elevation on A4.7.

4. *A preliminary meeting was held with the Fire Marshal, where egress deficiencies were noted in the basement level, which have not been addressed.*

C3 Response: 12/02/2021: Revised plans show relocated stairs towards the SW of the basement layout, which address remoteness and travel path issues related in meeting.

Email Comments - HPB Zoning Review (11-24-21)

Reviewer: Irina Villegas

1. *Provide a narrative responding to staff comments.*

C3 Response: 11/29/2021: Confirmed, please see the following.

2. *Based on determination of frontage on Michigan Avenue, grade elevation shall be noted in the survey at the sidewalk on Michigan Avenue. Note that grade elevation shall be measured at the sidewalk at the center of the site*

C3 Response: 11/29/2021: The grade elevation at the center of the site closest to Michigan Ave is 3.7'. We will update the survey to include the sidewalk elevation for building permit.

3. *Page A2.1. Revise grade elevation to match elevation noted in survey, as noted above.*

C3 Response: 11/29/2021: The grade elevation is approximately 4', where survey shows closest as 3.7' NGVD. We will update the survey to include the sidewalk elevation and revise accordingly for building permit.

4. *Page A2.5. Stairs, bathroom count in FAR. Identify room next to the stairs on the north side.*

C3 Response: 11/01/2021: The bicycle parking is below grade and the ceiling of the enclosure is below grade. The ceiling of the bathroom is below grade. The bathroom ceiling is at approximately N.G.V.D +4', and for permitting will be below grade so these areas do not count as FAR. The elevator will be counted. All treads and risers of stairs that are above grade are accounted for on the ground floor plan for FAR. See section on page A4.12.

5. *Page A2.6. Area below the ground floor stair and covered steps above count in FAR on ground and mezzanine. Covered outdoor terrace counts in FAR.*

C3 Response: 11/29/2021: The area below Stair is Electric Room and relates to Transformer, which is a qualified exemption so it does not count towards FAR. The covered Terrace is past the structural columns, and has been verified by staff in previous meetings that it would not count as FAR.

6. *Page A3.2. Columns and walls shall be setback 18" from the two-way driveway for perpendicular parking. Project does not comply.*

C3 Response: 11/29/2021: The columns and walls have been revised and dimensioned 1'-6" away from the drive aisle on page A3.2.

7. *Page A4.12. As shown on plans, enclosed area in mezzanine adjacent to stair shall count in FAR. This area is not consistent with page A3.4 noted as open to below.*

C3 Response: 11/29/2021: Please see revised A4.12. This area was labeled incorrectly; it is an architectural ceiling of lobby; a cavity space, and is not occupiable.

8. *Page A4.13. Enclosed area in mezzanine adjacent to elevator shall count in FAR. This area is not consistent with page A3.4 noted as open to below.*

C3 Response: 11/29/2021: Please see revised A4.13. This area was labeled incorrectly; it is an architectural ceiling of lobby; a cavity space, and is not occupiable.

9. *Page A4.14. Enclosed area in mezzanine next to canopy count in FAR. This area is not consistent with page A3.4 noted as open to below. There is no guard rail around the open courtyard area? Please clarify.*

C3 Response: 11/29/2021: Please see revised A4.14. This area was labeled incorrectly; it is an architectural ceiling of lobby; a cavity space, and is not occupiable.

10. *Show mechanical equipment height and screening on section drawings*

C3 Response: 11/29/2021: Please see revised A4.11 and 4.12.

11. *Project does not comply with section 133-61. a)(1) d. and f. Light poles and traffic light shall be relocated. There shall be 10'-0" of access without obstructions.*

C3 Response: 11/29/2021: The section notes, 'wherever possible' to provide area with no obstructions. We are maintaining consistency with Michigan Ave to the south but will propose re-location of the pole moving forward as necessary.

12. *Loading plan approved by the parking Department shall be provided to waive the required loading requirements. Indicate expected areas used for loading on the street (3 spaces required).*

C3 Response: 11/29/2021, updated 12/03/21: We are asking for waiver due to preserving the contributing structure and adaptive reuse of the existing foundation.

There is 1 extra-long Loading Spaced 5' wide on the property noted adjacent to the alley on page A3.1, accepted per conversation with Parking Director on 12/3/21. Fire Department to confirm access from alley.



Mon 12/6/2021 11:09 AM

Matthew Amster

RE: Staff Comments Related to Parking for 411 Michigan Avenue - PB and HPB Applications

To [Beltran, Monica](#); [Elan Blumberg](#); [Rivera, Michael](#)

Cc [Madan, Rogelio](#); [Garavito, Alejandro](#); [Maximo Polanco \(mpolanco@langan.com\)](#); [Deborah Tackett \(DeborahTackett@miamibeachfl.gov\)](#); [Seiberling, James](#); [David Butter](#)



Good morning Monica,

Thank you for meeting with us on Friday, 12/3 to discuss the project and specifically the loading operations. As discussed, the proposed development includes an office building with a minimal amount of ground floor retail (approx. 3,000 SF) and a small juice bar in a historic structure. A private drive will be located between the structures connecting the alley in the east to Michigan Avenue in the west. See attached ground floor plan. Notably all vehicular traffic will be managed as valet only and park either in the basement or in a parking structure on the south side of the private drive.

For loading operations, we do not anticipate that these uses will have frequent or large deliveries. We are providing an approximately 10' x 30' loading zone abutting the east alley with 5' of the width on the property and 5' in the 20' alley. This leaves 15' unobstructed width in the alley during a loading operation. We further discussed that small vans may deliver in the private drive and if needed, and so could larger trucks during off-peak hours.

You indicated that this loading operation was acceptable as long as the Fire Department evaluated that they have sufficient maneuverability during loading operations. We are reaching out to the Fire Department and will report back.

Please confirm this understanding and let us know if any question.

Thanks again,

Matt